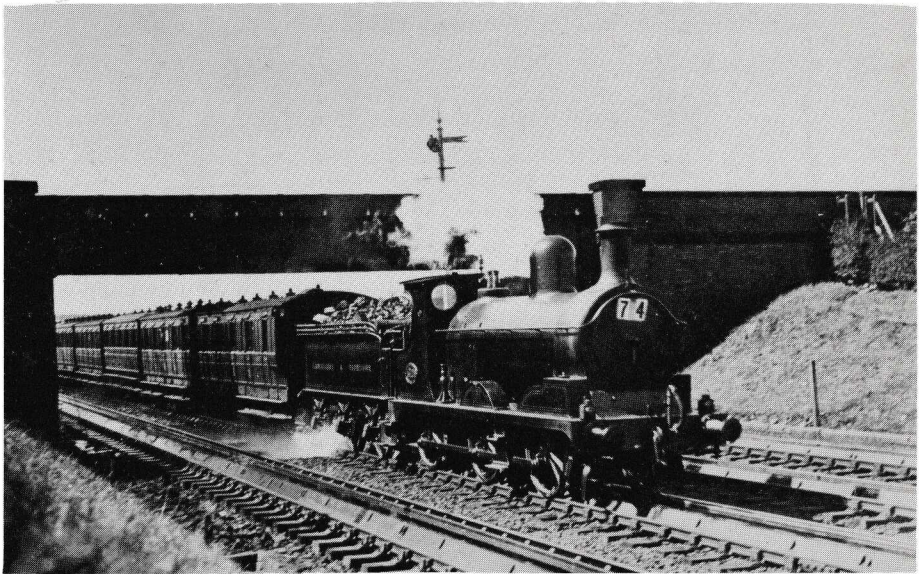


PLATFORM 23



THE JOURNAL
OF THE
**Lancashire & Yorkshire
Railway Society**

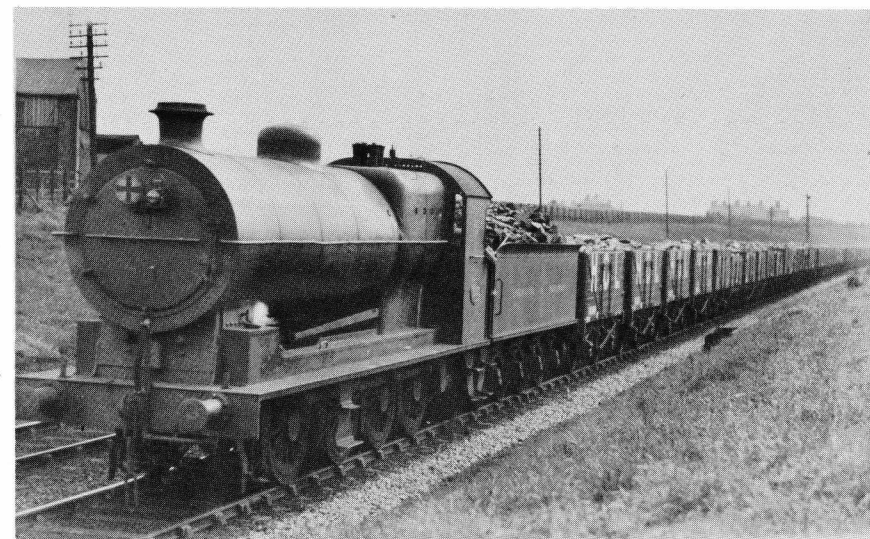
Platform 23 is the Summer 1987 edition of the L.&Y.R. Society Journal. For further details of the Society and its facilities, please contact the Hon. Secretary—Mr T. Wray, 17 Chale Drive, Middleton, Manchester M24 2BZ

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COVER PHOTOGRAPH:

The holiday season brought excursions to Blackpool from all over the industrial north. This train of old Manchester, Sheffield & Lincolnshire Railway six-wheelers has come from G.C.R. territory and is here returning towards Preston in the mid-afternoon. Much of this traffic was handled by the Aspinall six-coupled 'goods' locomotives and No.1126 shows little of her thirty-year age as she picks up water on Lea Road troughs. There were few places in the land that you could see an 0-6-0 picking up water at speed with 'express' head code but the LYR engines gave a good account of themselves on such duties and such was the density of traffic that the trains followed each other from section to section on summer Saturdays.

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“TAKING COALS TO NEWCASTLE” sic

Our centre page feature in the last journal illustrated a right-away goods that has caused much discussion amongst some members. Gordon Heywood played as a child in the fields around that very spot and the view was so familiar to him. The view to the south was however almost identical to the view in the other direction and the inevitable happened . . . we got the direction wrong. Therefore the train is travelling in a eastward direction, although at this point, the line is running due north. On top of this, a typesetting error put the gradient as 1 in 30 whereas it was 1 in 130 easing to 1 in 160 nearer to Middleton Junction. The angle of the sun places the time as sometime after mid-day. What is most confusing is that the train of N.E.R. 10½-ton coal wagons is fully loaded; truly a case of “carrying coals to Newcastle” or at least in that general direction.

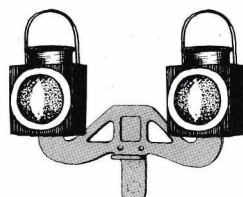
The train was obviously a regular working as there are others taken by George Smith at this location. Our heading photograph shows No.1553 on another occasion with what is likely to be the same working. It is a coincidence that one view (in Platform 22) shows No.1453 and here we have 1553 which was one of the first lot of brand new superheated 0-8-0s built in 1913. The train too is unusual as this one appears to be entirely private owner wagons. Both pictures portray wagons in ‘common user stock’ which was a method operated by the GWR, LNWR, LYR, MR and NER to ensure full use from wagon stock in the Great War period. All wagons of more than three-plank height (with certain exceptions) and including P.O. coal wagons were intensely used to obviate empty return working so there we have the reason for these two rather foreign rakes of coal wagons. In actual fact, the first three wagons behind 1553 are owned by H.G.Lewis of Cardiff. This firm quickly grew from small beginnings and hired

A Bradford/Leeds express passes through Moston station on the climb out of Manchester. The hoarding advertises 'valuable freehold building site to be sold' and the fields behind the railway became the factory of A. V. Roe. The whole area has been fully built-up since the photograph was taken but when George took this view, it was open fields although less than five miles from Manchester Victoria.



out wagons all over the country. They later became Hall Lewis and kept on growing only to go out of business quite suddenly . . . it has been suggested that the company over-reached itself. The three wagons look to be in brand new condition and the leading one carries the number 14243 but that does not mean that there were fourteen thousand previous wagons. Most of the larger users commenced their number series with a suitably high figure as a mark of self esteem!

Philip Holroyd remembers his father who was a signalman at Halifax East Jct. box in pregrouping days talking about the unusual double lamp holders shown on the engine. These were specially used on 'Right Away' freight trains as explained in the last Journal; the men called them CATS EYES brackets, for obvious reasons.



As a contrast to all these pictures of goods workings, we reproduce another of George Smith's photographs taken in the area. This one is looking toward Manchester about 1924 and shows a Hughes 4-6-0 in the crimson livery of the LMS. It would have had the number on the tender at that period with the crest of the new company on the cabside but the engine has changed tenders. The dirty, black-liveried tender adds to the anonymity of the engine and the only mark of a running-number is on the smokebox numberplate which is unreadable in the shadows. No.10418 ran in this manner around this period and this could be another view of it.

Mr Joseph Ashton, of Wigan, a signalman on the L.Y.R. has been presented with the medal of the Order of the British Empire for his services in connection with the enemy air raid on Wigan on the night of 12th April 1918. A bomb plunged through the roof of the signal cabin, crashed through the floor and buried itself in the cellar. Mr Ashton carried on duty until he was relieved three hours later.



MELTHAM



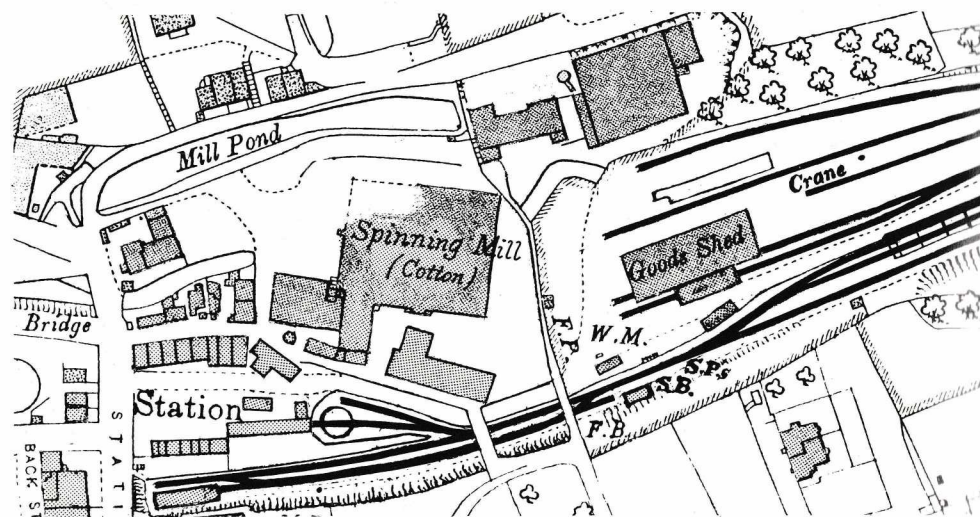
Dr. A. EARNSHAW

THE RECENT PUBLICATION of the excellent book by the Society, on the Meltham Branch, prompted me to look out some of my old research material from a booklet I wrote about the branch a few years ago. Coupled with other information that has recently come to hand, and been confirmed by records from the Ordnance Survey, an entirely new outlook comes on the early days at Meltham.

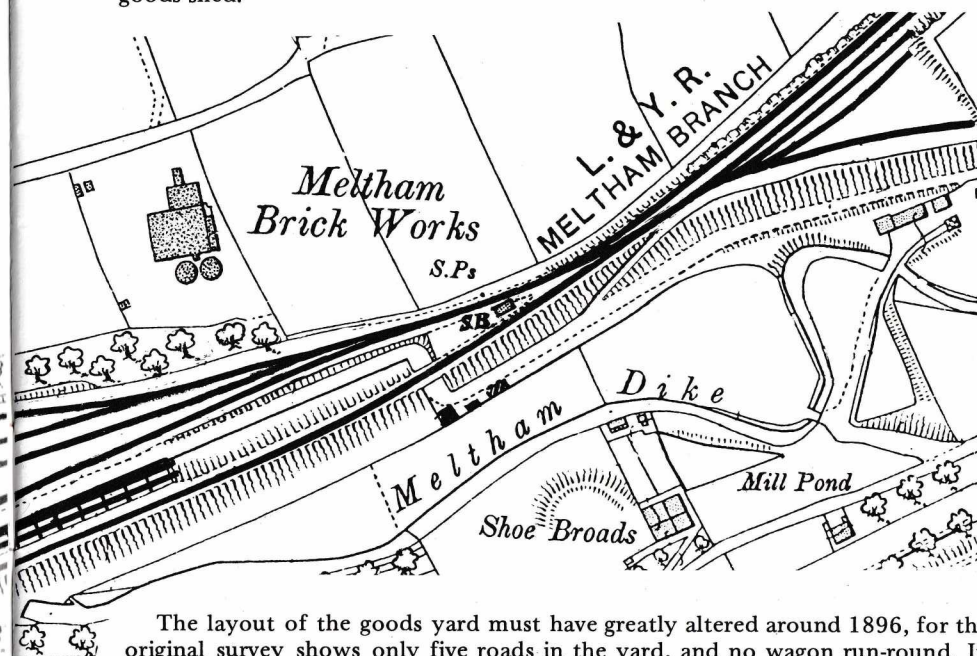
Following the opening of the line in 1868-9, the branch terminus was laid out completely different to the design which is known to have existed from about 1896 onwards.

The original signal cabin was located in the 'V' of the junction of goods yard and passenger line until replaced by a new box in 1896. Coal staithes, possibly 10 or 11 in number, were served by two roads, the access to which faced the station buildings. About 1894-5 the staithes were singled to just one road, there being some local rumour that the timbers were already deteriorating, and this may be true as it is reported that staging and supports were brought from another location and were second-hand timbers.

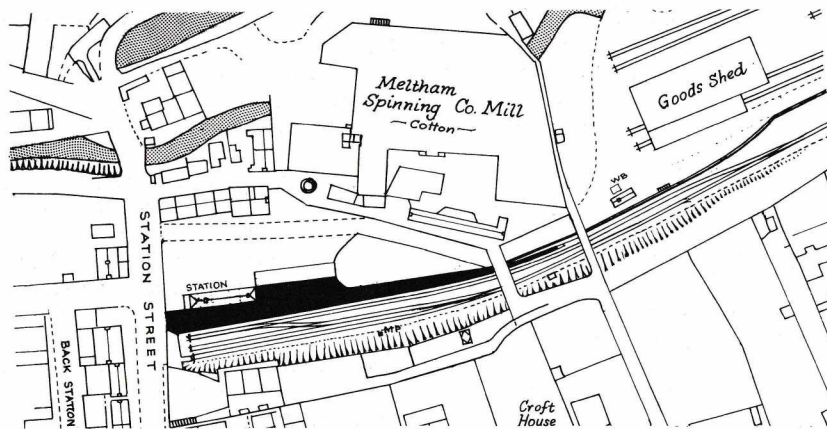
The platform layout was very much different with just two roads which split from the single approach road, the point being located just east of the metal footbridge, near what is described as a signal box. Whether this was a full box or a ground frame is not certain but available records again show it to have been closed in 1897. The branch was worked as one engine in steam and staff until the new Meltham 'Goods' box was opened in 1896. In the absence of telegraph and signalling it must be assumed that the 'Meltham Station' box was a most basic affair.



Beneath the wooden footbridge were the points for the engine shed where there was a turntable and a small coaling siding. It is also suggested that there may have been a small carriage shed at Meltham, as the early maps show a building located at the end of the run-round road at Meltham. There is no recollection of a building at this location by my father, so that would date it to having been removed at least before the turn of the century. Through copies of the survey of 1880, 1888, 1891, 1894, and 1906, we have been able to date the closure of the engine shed to within just a few years. The engine shed is shown as connected to the line in 1880, but on the survey of 1888 it is not. The same maps show the coal staithes having two roads (1880), then one road (1894), and no roads (1906). The photograph taken in Station Street (page 20) and showing the engine shed roof, can be fairly accurately detailed to have been taken about 1882-3. The print shows a local carter and haulier from Meltham, who was my great-grandfather, and who operated from an office near the coal staithes at Meltham Station. The young boy sitting on the cart is my grandfather, and from other family pictures we would suggest this picture was taken within the time period quoted. It is of interest to note that our family records show a removal of the business offices in the station yard in 1896, moving from by the coal staithes to near the goods shed.



The layout of the goods yard must have greatly altered around 1896, for the original survey shows only five roads in the yard, and no wagon run-round. In the survey of 1888 it had increased to five sidings and a run-round, and by 1906 it had increased to seven and a run-round. It would appear that prior to 1896 there was a small three-road yard to the east of the station throat, located on an area of flat ground that always looked as though it was made for railway sidings. Again from family knowledge I would suggest that this area was originally used as a private sidings for Jonas Brook & Bros, one siding being used for timber, and the double sidings alongside the main line probably used for cotton traffic.



The track layout as it was by the turn of the century. The Spinning Company Mill was completed in 1887 and the 1880s map on the previous page shows it under construction at that time.

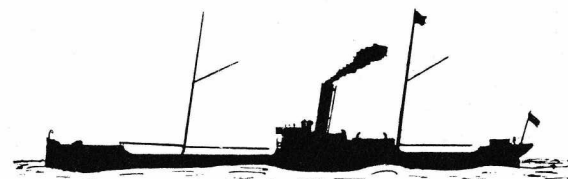
The Meltham Mills connection is certainly worth recording, for it was mainly on the instigation of the Brook family that the line to Meltham was built. Charles Brook Jun, it will be recalled, actually cut the first sod near Meltham Mills. Meltham Mills were cotton thread-spinning mills, complete with their own bobbin manufacturing plant. They lay about half a mile from Meltham in a small valley along the Royd Beck. It was originally intended that a spur be built to the mills, leaving the branch somewhere near Hall Heys Wood, and about half a mile from Healey House Station. The spur was eventually rejected by the L.&Y.R., and in a letter seen by the author, they wrote to Brooks quoting unacceptable gradients and expensive earthworks as the reasons for their change of plans. Local conjecture was that the company had become so heavily committed in other areas that it was unable to proceed with the planned spur. Needless to say the directors of the company were furious, particularly in view of the support they had given the L.&Y.R.

A compromise was put forward by Charles Brook Junior, though it is reasonably safe to conclude that the suggestion originally came from the railway company. This involved construction of goods facilities at Meltham solely for the company's use, and a passenger station at Spinks Mire. The station was to be an unstaffed halt, with tickets being issued at the mill office. Ticket equipment was issued to the company, with the stipulation that tickets be supplied only to the workers of Jonas Brook & Bros. However, Elishah Brook objected strongly, and company pensioners were allowed to buy tickets, this facility later being extended to allow local residents to purchase tickets at the mill. Cartage from Meltham Station Yard to Meltham Mills would involve a lengthy climb up Station Street, then via the centre of the village, and down a long descent to the mills. Brooks were again unhappy about this feature, and in turn forced the company to build a special road from the yard to the mills. This road left the yard and passed under the passenger line, then continuing alongside Hall Beck to Spinks Mire Mill (Quarmby & Sykes). Near there it joined onto the turnpike, just short of Meltham Mills Bar House. It then turned along a short section of Meltham Mills Road before entering Brooks mill. Due to the large quantities of coal carted along this

road, the cartway became known as 'The Coalgates'. Between Bridge House and the mills the whole of the cartway was practically level, the only steep section being from the bridge up to the goods yard. Two hauliers worked the majority of loads along the road, these being Haigh & Sons, and Earnshaw Bros & Sons.

Major changes took place at Meltham in the mid-1800s: waterworks committees, public health boards, and the gas works were all introduced. The changes in the railway of the mid-1890s may be considered significant when compared with contemporary changes taking place in Meltham at the time. Previously, affairs had been conducted by a town board, but on 31st December 1894 the Urban District Council was formed. Around this time Edward Brook gave £1,500 to build the council offices, which were constructed on my grandfather's land. In 1896 the Urban District of Meltham (extension) Order was approved, and around this time there seems to have been major changes within the district. Improvement of road and rail facilities were an early priority for the authority, and though there is no direct account to relate the changes on the Meltham Branch to council policy, it is more than likely there was a very definite connection between the two occurrences. Fortunately, proposals for a tram route to Meltham (1902-9) did not come to fruition, the Huddersfield Corporation being reluctant to operate trams up the Big Valley at Netherton.

Flags and Funnels of the L.&Y.R. Shipping Services



D. O. KING

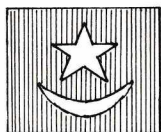
BY THE EARLY PART of this century the Lancashire and Yorkshire Railway had become heavily involved in shipping, serving places as far afield as the ports of Southern Ireland and Copenhagen. So with a combination of sea and rail they could offer the longest journey of any Railway Company in the Country. The vessels were based at the ports of Liverpool and Fleetwood on the West Coast and the Humber ports of Goole and Hull in the East. The fleets of each port had their own distinctive flags and funnels, which were based on the markings of the founder companies.

The Drogheda Steam Packet Company ran steamers from Liverpool to Southern Ireland. The vessels had plain black funnels and it is known that in 1882 the house flag was the white star and crescent moon of the City of Drogheda on a red background; by 1895 the flag had become green. When the Lancashire and Yorkshire railway absorbed the company in 1902 they added a yellow band to

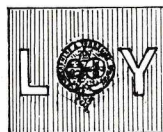
LIVERPOOL



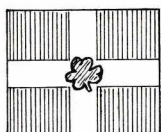
Drogheda Steam Packet Co.



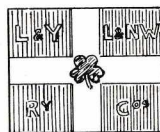
Lancashire & Yorkshire Rly.



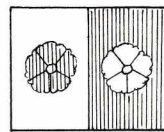
FLEETWOOD



(A)



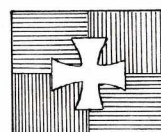
(B)



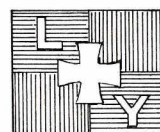
(C)

- (A) F. Kemp and Company
(B) LYR/LNWR Joint Services
(C) Lancashire & Yorkshire Rly.

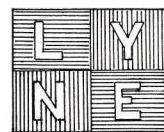
GOOLE/HULL



(D)



(E)



(F)

- (D) Goole Steam Shipping Co.
(E) Lancashire & Yorkshire Rly., Goole
(F) LYR/NER Joint Service, Hull



Red



Blue



Buff

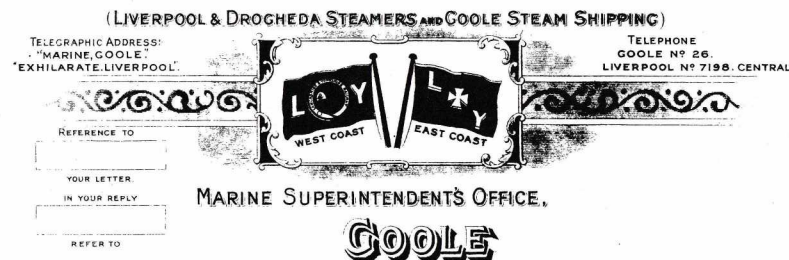


Yellow

the funnel. They also reverted the house flag back to red, but replaced the star and moon with their company crest. This was on a gold field and enclosed by a white garter carrying the company title. The initials 'L' and 'Y' were positioned either side of this emblem.

From 1843 Fleetwood to Northern Ireland services had been operated by F. Kemp and Company's North Lancashire Steam Navigation Company. Kemp's vessels had black funnels with a narrow white band and the house flag was red with a shamrock at the centre of a white cross. In 1870, as a joint venture, the L.N.W.R. and the Lancashire and Yorkshire Railway took over these services and adopted Kemp's colours. The only alteration was to add their initials to the flag. These were white and divided out into all four cantons. For their own Fleetwood vessels, the Lancashire and Yorkshire Railway also used the same funnel colours, but had their own individual house flag. This was halved vertically with a white and red section which had contrasting red and white roses of Lancashire and Yorkshire on them.

The origin of the Lancashire and Yorkshire Railway's Goole service was the Goole Steam Shipping Company whose vessels were transferred to the Railway's ownership on 1st January 1905. For advertising purposes the old company's image and title were preserved, as were the colours. The funnel was buff with a black top separated by a red band which gave rise to the colloquial name for the fleet as the 'soot, blood and suet' line. The house flag was quartered blue and red with a white Maltese cross in the centre. The Lancashire and Yorkshire Railway added the white initials 'L' and 'Y' to the blue sections. In 1906 the S.S. Duke of Clarence was transferred from the Fleetwood fleet to run a summer passenger service from the Riverside Quay, Hull to the Belgian port of Zeebrugge. A year later the North Eastern Railway, the operators of the Riverside Quay, became partners in the service. The vessel's funnel was painted in the colours of the Goole fleet, but for the joint service there was a new house flag. The Maltese cross was removed and an initial from the titles of both companies added to each quarter in white.



The coloured letterhead from 1905.

J.B.H. collection

Claims have been made that the L. & Y.R. steamship 'ALT' has sunk a German submarine. If the claim is proved, the officers and crew will be entitled to the award of £500. The paper 'Syren and Shipping' understands that the 'Alt' has since been examined in dry dock at Holyhead by Admiralty officials and the result of their investigations is awaited by the editor.

Manchester Guardian

5th August 1915

L. & Y.R. SIGNALLING

by T. T. SUTCLIFFE & F. COLLINGE

PART 1

For those not familiar with signalling practices, we have selected several boxes with interesting track layouts and will try to explain the function and operation of the various signals, levers, and associated interlocking. The diagrams, which are not to scale, are copied from those held in the MT6 files at the Public Record Office, Kew. MT6 covers most opening signalling alterations, new signal boxes and layouts from 1840 to 1919. Many of these files have proved to be very useful as an aid in the production of the "Traffic Control Maps" books. These documents must be read as a statement of intent, as the inspector sometimes made recommendations which were not subsequently recorded on the diagrams. Anyone visiting the P.R.O. and drawing out these files must read the inspector's reports enclosed with the diagram.

ASTLEY BRIDGE JUNCTION of 1875

This was a new signal box located in a most awkward and unique position high above ground level on viaducts where the Astley Bridge branch left the main Bolton to Blackburn line. The timber box was supported on girders in the fork of the diverging lines and viaducts, and was probably erected by the Manchester firm of E. S. Yardley.

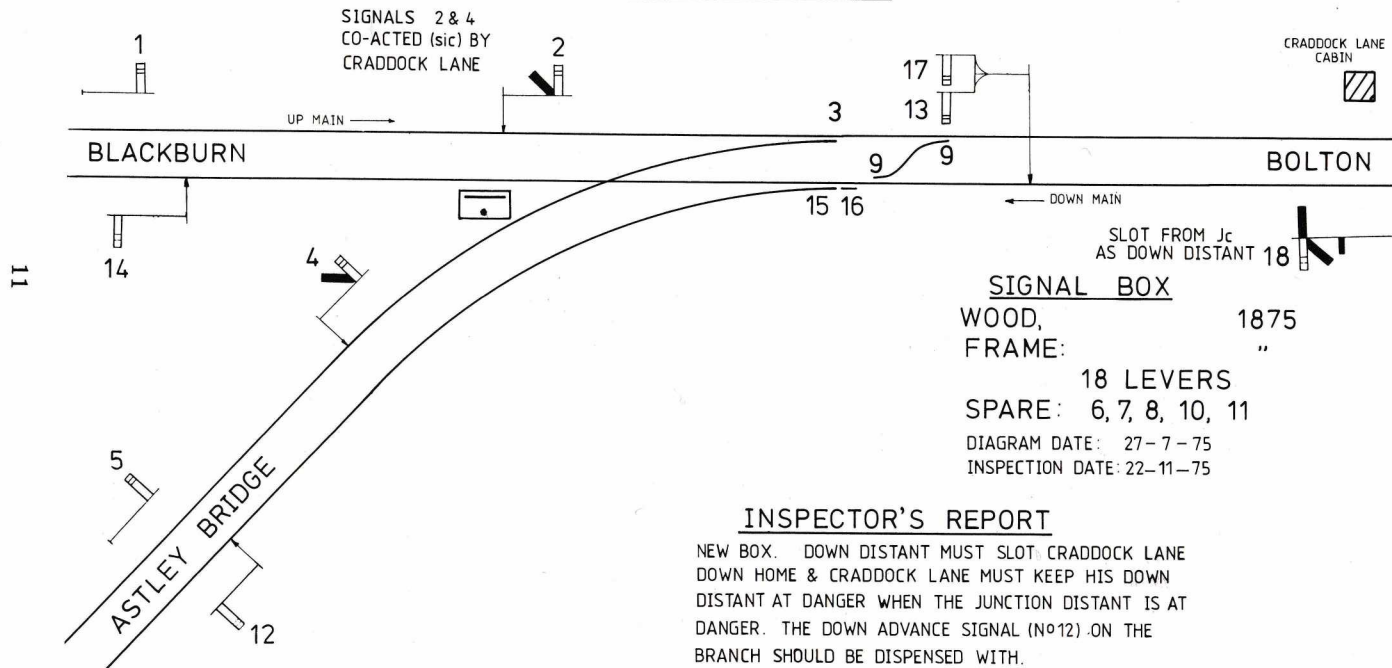
At this time, the distant signals were generally of identical appearance to the home and starting signals. The arms were painted red with a white stripe and a single red glass in the lamp spectacle. A red light was shown at night when the arm was at danger, but when the arm was 'pulled off', the red glass would move clear of the lamp to show a white light. The poor driver simply had to know the road and which signal was which. However having said this, signal arms with notched ends were also in use at this period (See photograph in Platform 9, page 18). Whether this was being used as a distant signal or for some other purpose is not known at present. Distant signals may be passed at danger, and serve as a warning for the driver to slow down and be prepared to stop at the home signal ahead. If clear, they indicate that all the stop signals ahead worked from the same box and referring to the same line are also in the clear position, and that the train may continue at normal speed. Distant signals were usually placed about 600 to 800 yards before reaching the home signal depending on sighting, gradient, and line speeds, the idea being to give the driver of the fastest train ample distance in which to bring his train to a stand at the home signal after sighting a distant at danger. In the case of Astley Bridge Junction, the line is on a rising gradient from Bolton, and this permitted No.18 distant to be placed much closer to the home signal (13/17), than was the case with the up-signals 1 and 2.

At the half-yearly meeting held on Wednesday 11th August 1875 it was stated that 170 miles of the railway was covered by the block system and 130 miles in progress of installation, with 80 miles under consideration.

Manchester Guardian, 12th August 1875.

147/23

ASTLEY BRIDGE Jc



SIGNAL BOX
 WOOD, 1875
 FRAME: "
 18 LEVERS
 SPARE: 6, 7, 8, 10, 11
 DIAGRAM DATE: 27-7-75
 INSPECTION DATE: 22-11-75

INSPECTOR'S REPORT

NEW BOX. DOWN DISTANT MUST SLOT CRADDOCK LANE
 DOWN HOME & CRADDOCK LANE MUST KEEP HIS DOWN
 DISTANT AT DANGER WHEN THE JUNCTION DISTANT IS AT
 DANGER. THE DOWN ADVANCE SIGNAL (Nº12) ON THE
 BRANCH SHOULD BE DISPENSED WITH.

JJS
 10/86

The levers working the distant signals (1, 5, and 18) were probably painted leaf green. Green continued to be used for this purpose until the 1920s when it was replaced by yellow, round about the same time that the red distant signal spectacle glasses were replaced by yellow. The interlocking mechanism prevented a distant signal lever being pulled unless the home and starting signal levers for that line had first been pulled. At Astley Bridge Junction, lever No.1 would be locked in its normal position until No.2 had been pulled; and when No.1 was pulled it would prevent No.2 being replaced. Levers 5 and 18 were similarly locked by 4, and 14 and 17 respectively.

The other levers in the frame were also painted distinctive colours according to their functions. The home and starting signal levers (2, 4, 12, 13, 14, and 17) would be red; point levers (3 and 9 and 15) were black, and the facing-point-lock lever (16) would be blue. Spare levers were white. This colour scheme was certainly used in later years but as we do not know when it was introduced, we cannot be certain that it was in use when Astley Bridge Junction box opened in 1875.

Interlocking also existed between the junction point levers (3 and 15). No.3 could not be pulled to let a train come off the branch until No.16 had first been pulled. This provided a safeguard against a down-train over-running signals, as it would then be diverted onto the branch instead of possibly colliding on the diamond crossing of the junction. As a further safeguard, lever No.16 worked a bolt which secured the facing points (15) in either position. To prevent any chance of these points being moved under a train, a lifting-bar was provided, working in conjunction with the bolt mechanism worked by lever 16. The bar was close to the inside of one of the running-rails in such a position that the flanges of any wheels would prevent it being raised, and so preventing the facing points being unbolted when a train was on them. The rest of the interlocking was fairly simple. Pulling No.9 crossover points locked signals 2, 4, 13, and 17 at danger. Signal lever No.4 required that 3 be pulled (which itself required 15 to be pulled). No.2 could only be pulled if No.3 was normal. In the down direction, No.13 required points 15 to be pulled and then bolted by pulling No.16. No.17 simply required No.16 to be pulled to bolt the points in their normal position.

One unusual feature of the layout is the close proximity of the adjoining box at Craddock Lane. This led to the necessity of controlling some of the signals from both boxes. On the diagram, all the arms shown as solid black are ones worked from Craddock Lane box. No.18 signal was on the same post as some other arms, which was a commonly used economy in the early days of signalling.



The arm shown here on the right was the up-home for Craddock Lane. The small lower arm on the left was a shunt-ahead arm for down trains to draw past the signal so as to be able to set back into Craddock Lane sidings. No.18 signal for Astley Bridge Junction is shown as being slotted with Craddock Lane's down home signal. The B.O.T. Inspector's report says that this signal was 'co-acted' by Craddock Lane, but this terminology does not have the same meaning as it does these days. See Note at end of this article. The actual arrangement of this signal 18 is shrouded in some doubt. If it consisted simply of one arm acting as Craddock Lane down-starter and slotted as Astley Bridge's down-distant, the problem is that it would never be possible to clear it unless the home and starting signals at Astley Bridge Junction were also clear. This obviously would not always be the case, and we must look for some other explanation. An

alternative arrangement could have been the provision of two arms (one as Craddock Lane starter, and one as Astley Bridge Junction distant) mounted close together and slotted like modern home and distant arms on the same post, or even two arms on a common pivot. The illustration of the early signal at Liversedge (Platform 9, page 18) appears to show a signal of this sort in the 1870s. There are two separate spectacles and lamps facing the same way, but very close together. Unfortunately, the photograph does not show us the top of the signal, nor any slotting mechanism. It is of interest to note the notched end of this arm, and the straight white band. Maybe the L.&Y.R. was one of the first users of notched distant arms. Whatever the precise arrangements, similar slotting existed on the up home signals (2 and 4) at Astley Bridge Junction.

The 1875 Astley Bridge box closed in 1913, and was replaced by a new L.&Y.R. box located in the same precarious position in the angle between the two viaducts. By looking through the gaps in the locking room floorboards, you could see the cobbled street about 100 feet below! Thankfully, this box closed in 1981 and was demolished in 1984. The next part of this article will describe the arrangements at the 1894 box at Newsholme.

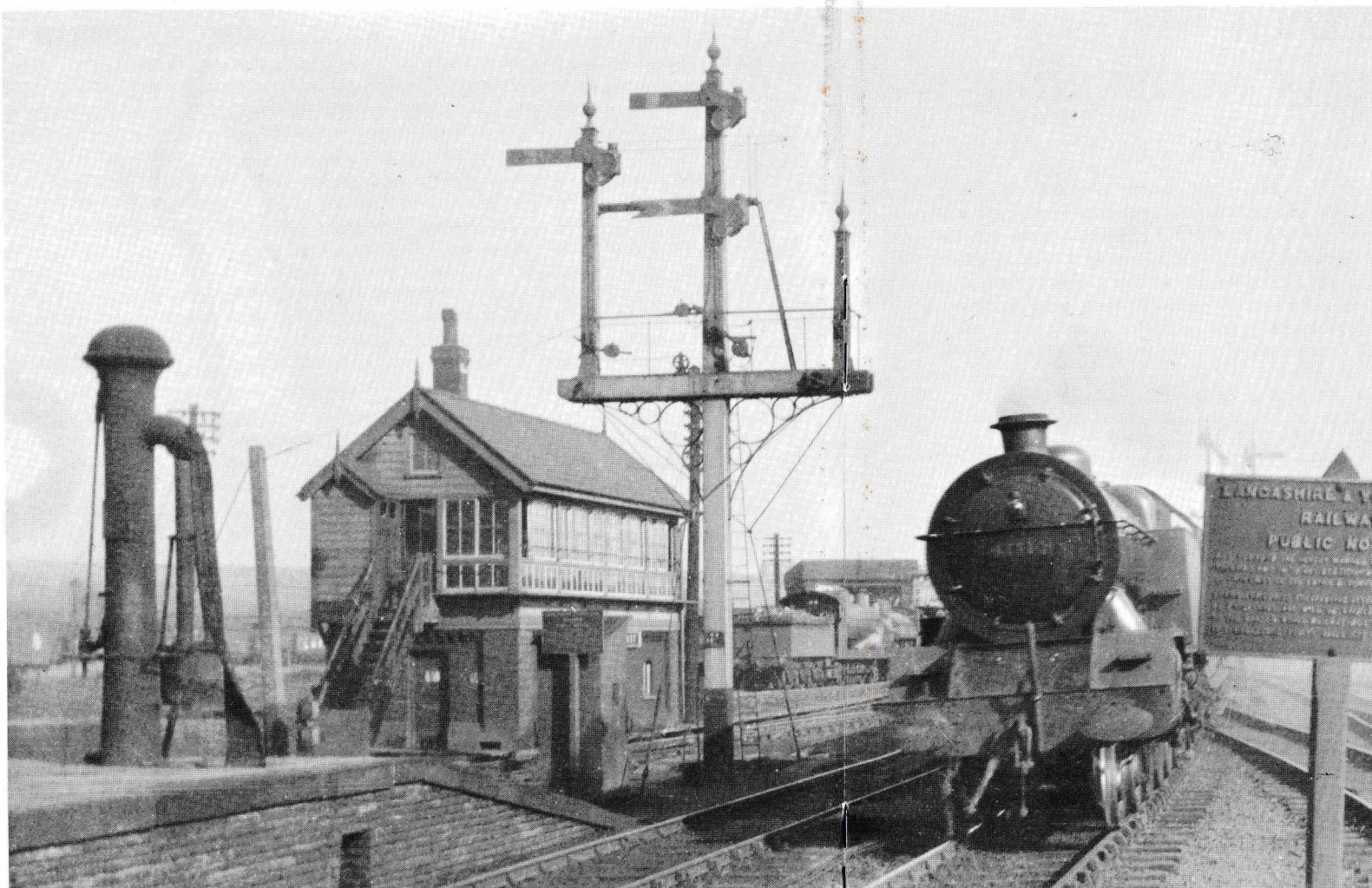
Note: 'Co-acting' now refers to the practice of working two arms simultaneously by one lever. The arms are usually located at different heights on the same post to overcome sighting problems near bridges and station canopies etc. In very exceptional cases, separate posts have been used on opposite sides of the line.



TIE-ON LABELS

Tie-on Labels are supplied by Stores Department, to be attached to packages which cannot be stamped or labelled. These must also be used for Fish and Perishable consignments under 2cwts., and packages to which stamps or labels cannot be securely affixed.

—L.&Y.R. Parcels Traffic Instructions 1911



LOW MOOR

*photograph by the late
A. P. Herbert*

courtesy Ron Herbert

As this photograph was taken in the late 1920s, I apologize for it being an L.M.S. period view in our pregrouping journal. On the other hand, it shows very well how little had changed under the new management in the first few years.

The locomotive is No. 11114 of George Hughes's handsome and capable 'Baltic' tank class. This was the engine that was taken straight out of service in its crimson livery and sent for display at the 1925 Wembley Exhibition. There is no way of being sure whether the photograph was taken before or after the event. It is entering Low Moor station with a Bradford to Manchester service stopping train.

The signal box is Low Moor No. 2 East, having 52 levers and dating from 1900. The provision of an integral lavatory was a feature of only the larger boxes of the day but everything else is typical of the later design of cabins. The bracket signal dates from roughly the same period, being Raynar Wilson's earlier design of arm (note how the lamps are lower than the

later type in the far right) and the wrought iron 'hoop' pattern brackets. The right hand doll carried the starter for the GNR branch but it has now been removed. The remaining arms have not been repainted for several years as the distant arm is still in the red with a white chevron of pregrouping years. Yellow was adopted just before the grouping.

The water column is number 407 and has a brazier in attendance which was a familiar sight until the end of steam on British Railways. The stationmaster was responsible for seeing that the water columns did not freeze up. The water tank above the coaling stage in the centre background would probably have gas jets as a more efficient means of stopping a freeze-up but this was restricted to the larger water tanks only.

Finally, the only evidence of the LMS, other than the livery of the engine is a distant coal wagon just behind the signal box. It alone shows the title of the new railway company.

Barry C. Lane

L. & Y.R.

WATER COLUMN NUMBERING

G. H. Foxley

FOR SOME TIME it has been desirable to know the system of numbering L & Y water columns and it has been possible to elucidate the sequence from copies of the List of Places where Water Columns and Troughs are Situated from the Appendices to the Working Time Table dated April 1902, January 1908 and January 1921 provided by T.T.Sutcliffe and G.Hallos. These lists are alphabetical so that the pattern of numbering is not immediately obvious; they were therefore rearranged placing the columns in numerical order; only the January 1921 list is reproduced here. All the lists show unused numbers and un-numbered columns.

SUMMARY OF THE THREE AVAILABLE LISTS

| <i>Date of list</i> | <i>Highest Number</i> | <i>No. of unused numbers</i> | <i>Un-numbered Columns</i> | <i>Total Columns</i> |
|---------------------|-----------------------|------------------------------|----------------------------|----------------------|
| <i>April 1902</i> | 382 | 18 | 31 | 395 |
| <i>January 1908</i> | 474 | 46 | 18 | 446 |
| <i>January 1921</i> | 555 | 84 | 18 | 489 |

The basic pattern revealed by the 1902 list was as follows: column No.1 was at Salford and the Lancashire part of the system was numbered first, ending at Newton Heath with No. 241. The Yorkshire numbering started with No. 242 at Bradford and ended with No. 325 at Barnsley; later numbers were randomly spread, mainly over the Lancashire area. The apparent anomalous use of parachute No. 290 at Blackpool (Talbot Road) arises from the transfer of this column from Lockwood in 1897; the replacement installed at Lockwood was No. 348.

Some of the increase in unused numbers in the 1908 list arose from the alterations at Manchester Victoria in 1903; one of the removed columns, No. 35, was re-installed at Blackpool (Talbot Road). Bolton, Wigan and Accrington also lost several columns in the period 1902-1908.

Non-used numbers in the two early lists probably represent columns which have been removed for one reason or another; the gaps in the 1921 list may not have been used and a list to bridge the 13-year gap between 1908 and 1921 would be useful. Occasionally a redundant column was re-installed elsewhere on the system, e.g. Nos. 35, 49, 290, but in the main, additions and replacements received new (higher) numbers e.g. No. 344 was replaced by No. 412.

Note: The water capacity of L&Y tenders was rather low (1,800-2,500 gallons) although the tenders of 0-8-0 coal engines had increased capacity (3,600 gallons). The 2-4-2 tanks carried 1,340 or 1,540 gallons. A very rough average of water consumption would be about 30 gallons per mile but clearly this would depend on how hard the engine was worked, engine type etc. There was thus the need for frequent supplies of water hence the large number of water columns (and troughs).

L.Y.R. January 1921 List of Water Columns

P = Parachute

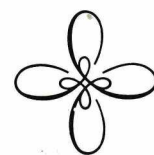
N.A. = Not allocated

| REGISTER No. | PLACE | WHERE SITUATED | REGISTER No. | PLACE | WHERE SITUATED |
|-----------------|-----------------------------|---------------------------------------|-----------------|--------------------|---------------------------------|
| 1 | Salford | West End of Platform. | 49 | Horbury and Ossett | Goods Yard, West End. |
| 2 | Salford | Goods Yard. | 50P | Bolton (Loco) | Down Goods Loop. |
| 3P | Salford (Oldfield Rd. No.2) | Goods Line. | 51,52 | Bolton (Loco) | Loco Yard |
| 4 | Salford (Hope Street) | Sidings. | 53,54 | Bolton (Loco) | Loco Yard |
| 5 | Salford (Windsor Br. No.2) | Sidings. | 55 | Bolton (Loco) | Loco Yard |
| 6 - 10 | Agecroft | Loco Yard. | 56P | Moses Gate No.2 | Up Loop. |
| 11,12 | Bury (Bolton Street) | Loco Yard. | 57P | Moses Gate No.1 | Platform End. |
| 13 | N.A. | | 58 | Kearsley Junction | Down Sidings. |
| 14 | Bury (Bolton Street) | North End. | 59,60 | N.A. | |
| 15,16 | Bury (Knowsley Street) | Platform End. | 61P | Craddock Lane | Goods Yard. |
| 17 | Bury (Knowsley St.) Goods | | 62P | Bolton | Bullfield East Goods Yard. |
| 18 | Heap Bridge | Goods Yard. | 63P | Bolton | Bullfield West. |
| 19 | Heywood | Goods Yard. | 64 | Lostock Junction | Platform End (Middle Platform). |
| 20P | Ramsbottom | North End. | 65 | Lostock Junction | Platform End. |
| 21P | Ramsbottom | Down Goods Loop. | 66 | Blackrod | End of Platform. |
| 22P | Rawtenstall | West End of Platform. | 67 | Blackrod | End of Platform. |
| 23P | Bacup | End of Platform. | 68,69 | Horwich | Loco Yard. |
| 24,25 | Bacup | Loco Yard. | 70P | Horwich | Station. |
| 26,27,28 | Manchester (Victoria) | Bay Platforms. | 71 | Preston (L.&Y.) | Turntable Road. |
| 29P | Manchester (Victoria) | East End. Turntable Road. | 72 | Preston (L.&Y.) | Goods Yard. |
| 30 | Manchester (Victoria) | West End. No.14 Platform. | 73 | N.A. | |
| 31,32 | N.A. | | 74 | Preston (L.&Y.) | North End of Platform. |
| 33,34 | N.A. | | 75 | Preston Junction | End of Platform. |
| 35 | Blackpool (Talbot Road) | Loco Sidings. New Turntable Road. | 76P | Preston Junction | West End of Platform. |
| 36 | Manchester (Victoria) | East End. | 77P | Preston Junction | Ticket Platform. |
| 37P | Manchester (Victoria) | Red Bank Carriage Sidings. | 78P | Lostock Hall | Close to Up Home Signal. |
| 38 | N.A. | | 79P | Lostock Hall | Sidings. |
| 39 | Oldham Road | Top Level. | 80,81 | Lostock Hall | Loco Yard. |
| 40P | Miles Platting | Turntable Road. | 82,83 | Lostock Hall | Loco Yard. |
| 41 | Manchester (Victoria) | Newton Sidings. | 84,85 | Lostock Hall | Loco Yard. |
| 42 | Miles Platting | Brewery Sidings. | 86 | Lostock Hall | Near Loco Coal Stage. |
| 43P | Miles Platting | Between Down Goods Line and Siding at | 87 | Ormskirk | South End of Platform. |
| 44P | Stalybridge | Station [Philips Park Junction. | 88P | Ormskirk | North End of Platform. |
| 45,46 | N.A. | | 89 | N.A. | |
| 47,48 | N.A. | | 90,91 | Ormskirk | Skelmersdale Branch. |

| | | |
|---------|---------------------------|--|
| 92,93 | Orrell West | East End. |
| 94P | Wigan | Main Line from Liverpool. |
| 95P | Wigan | West End of Station. |
| 96P | Wigan | West End. |
| 97 | N.A. | |
| 98 | Wigan | Station. |
| 99 | Wigan | Station. |
| 100P | Wigan | No. 1 Exchange Sidings. |
| 101P | Wigan | Main Line from Southport. |
| 102 | Wigan | Loco Yard. |
| 103 | N.A. | |
| 104 | Bradford | Broomfield Sidings. (Water Tank). |
| 105 | N.A. | |
| 106 | Wigan | Loco Yard. |
| 107,108 | Appley Bridge | Platform Ends. |
| 109,110 | Southport | Loco Yard. |
| 111,112 | Southport | Loco Yard. |
| 112 | Whitley Bridge | Outside Up Loop. Listed T112 in 1908 List. |
| 113 | Southport | Loco Yard. |
| 114,115 | Southport | Platform Ends. |
| 116P | Hindley | Up Loop. |
| 117P | Hindley | Up Slow. |
| 118P | Chorley | South End. |
| 119P | Chorley | North End. |
| 120 | Chorley | Loco Yard. |
| 121,122 | Blundellsands | Platform Ends. |
| 123P | Kirkham | East End of Station. |
| 124P | Kirkham | West End of Station. |
| 125 | N.A. | |
| 126 | Blackpool (Talbot Road) | Loco Sidings. |
| 127,128 | N.A. | |
| 129 | Blackpool (Talbot Road) | Loco Sidings. |
| 130P | Blackpool (Talbot Road) | End of Platform. |
| 131,132 | Blackpool (Central) | Loco Sidings. |
| 133,134 | N.A. | |
| 135,136 | Fleetwood | Loco Yard. |
| 137 | Fleetwood | Loco Yard. |
| 138 | N.A. | |
| 139 | Fleetwood | Platform End. |
| 140,141 | Liverpool (Exchange Sta.) | Platform Ends. |
| 142,143 | Liverpool (Exchange Sta.) | Platform Ends. |
| 144,145 | Liverpool (Exchange Sta.) | Platform Ends |
| 146 | Accrington | West. |
| 147 | Liverpool (Exchange Sta.) | Platform End. |
| 148 | Sowerby Bridge | West End of Platforms. Up Loop Line. |
| 149 | Liverpool (Exchange Sta.) | Loco Sidings. |

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|---------|---------------------------|--|
| 150P | Sandhills | Platform End. |
| 151 | Sandhills | Platform End. |
| 152 | North Docks | High Level. |
| 153 | N.A. | |
| 154 | Sandhills | Loco Yard |
| 155 | Accrington | Up Platform. |
| 156P | Bankfield Goods | Canal Bridge. |
| 157 | N.A. | |
| 158P | Aintree Sorting Sidings | West Box. |
| 159P | Aintree Sorting Sidings | Between Nos 1 and 2 Boxes. |
| 160 | N.A. | |
| 161,162 | Aintree | Loco Yard. |
| 163,164 | Aintree | Loco Yard |
| 165 | Aintree | Loco Yard. |
| 166,167 | Accrington | North. |
| 168,169 | N.A. | |
| 170 | N.A. | |
| 171 | Bury (Knowsley Street) | Up through line. |
| 172 | Accrington | West. |
| 173 | N.A. | |
| 174P | Burnley (Bank Top) | South End. |
| 175P | Burnley (Bank Top) | North End. |
| 176P | Burnley (Bank Top) | Shunting Necks |
| 177P | Great Harwood | Platform End. |
| 178 | Great Harwood | Carriage Shed Sidings. |
| 179P | Blackburn | West End, Up Back Platform. |
| 180P | Blackburn | Up Bay Siding. |
| 181P | Daisy Field | Great Harwood Junction. |
| 182P | Blackburn Goods | Bolton Junction. 1908 List shows this at |
| 183 | N.A. | [Blackburn Station, Down Bay |
| 184P | Blackburn Goods | [Siding, No. 2 Bay. |
| 185P | Blackburn Goods | Goods Yard. |
| 186P | Blackburn | Goods Yard. |
| 187,188 | Blackburn Goods | Turntable Road. |
| 189P | Blackburn | North End. Up Goods Loop and Siding. |
| 190 | Blackburn Goods | End of Taylor Street Loop. |
| 191P | Darwen | Goods Yard. |
| 192-195 | Lower Darwen | North End of Platform. |
| 196P | Daisy Field | Loco Yard. |
| 197P | Clitheroe | North End of Station. |
| 198-200 | Hellifield | South End of Platform. |
| 201,202 | Burnley (Manchester Road) | Loco Yard. |
| 203 | Portsmouth | East End of Station. |
| 204 | Portsmouth | West End of Platform. |
| 205P | Todmorden | East End of Platform. |
| 206P | Todmorden | Up Goods Loop. |
| | | Hall Royd Junction. |

| | | |
|-----------|----------------------------|-------------------------------------|
| 207P | Todmorden | Burnley Fork. |
| 208P | Todmorden | East End of Platform. |
| 209P | Todmorden | West End of Platform. |
| 210P | Rochdale | Down Goods Loop, East Junction Box. |
| 211 | Rochdale | Up Goods Loop. |
| 212P | Rochdale | Facit Departure Platform |
| 213P | Rochdale | Oldham Departure Platform. |
| 214P | Rochdale | Manchester Departure Platform |
| 215P | Rochdale | Bury Departure Platform. |
| 216P | Rochdale | Turntable Road. |
| 217 | Facit | Platform End. |
| 218,219 | Castleton | Platform Ends. |
| 220,221 | Castleton (East Junction) | Sidings. |
| 222 | Castleton (South Junction) | Up Side. |
| 223P | Castleton (North Junction) | Down Side. |
| 224P,225P | Oldham (Mumps) | Platform Ends. |
| 226P | Oldham (Werneth) | Platform End. |
| 227P | Royton Junction | Up Sidings. |
| 228 | Middleton | Platform End. |
| 229 | N.A. | |
| 230-241 | Newton Heath | Loco Yard. |
| 242 | Bradford | Platform, West End. |
| 243 | Bradford | Platform, West End. |
| 244 | Bradford | Platform, West End. |
| 245 | Bradford | Platform End, No. 4. |
| 246P | Bradford | Goods Sidings. |
| 247P | Pontefract | Prince of Wales Colliery. |
| 248 | N.A. | |
| 249 | Low Moor | South End. |
| 250P | Low Moor | West End. |
| 251-258 | Low Moor | Loco Yard. |
| 259P | Halifax | South End of Platform. |
| 260P | Halifax | North End of Platform. |
| 261 | Halifax | Station. |
| 262 | Halifax | G.N. Passenger Station. |
| 263P | Halifax | Station. |
| 264P | Sowerby Bridge | East End of Platform. |
| 265 | N.A. | |
| 266P | Sowerby Bridge | West End of Platform. |
| 267 | N.A. | |
| 268 | Sowerby Bridge | End of Down Loop. |
| 269 | Sowerby Bridge | End of Up Loop. |
| 270P | Heckmondwike | Station. |
| 271-274 | Sowerby Bridge | Loco Yard. |
| 275P | Hebden Bridge | West End of Platform. |
| 276 | Hebden Bridge | East End of Platform. |



Many columns had a box where drivers of other than L.Y.R. trains had to place a 'Water Ticket'. A similar practice existed for L.Y.R. men running on foreign tracks). These were a common sight on water columns on the main lines and this example was on a parachute column at Rochdale 'Up' platform. The plate above it is a flat-backed example, being fastened by four screws onto the cast 'seat' in the column.

| | | | | | |
|---------|-------------------------|--|---------|-------------------------|--|
| 277P | Greetland | Platform End. | 337 | Manchester (Victoria) | Irwell Bridge. |
| 278 | Greetland | Platform End. | 338P | Chorley | Brook Street Bridge. |
| 279 | Greetland | Down Goods Sidings. | 339 | Wyre Dock Junction | Junction Sidings. |
| 280 | Brighouse | Goods Yard. | 340 | N.A. | |
| 281-286 | Mirfield | Loco Yard. | 341 | Shaw | Goods Yard. |
| 287 | Mirfield | Up side. | 342 | Westhoughton | Goods Yard. |
| 288 | Mirfield | Down side. | 343 | Nelson | Goods Yard. |
| 289 | Lockwood | North End of Station. | 344 | N.A. | |
| 290P | Blackpool (Talbot Road) | Goods Yard. | 345,346 | Ormskirk | Loco Yard. |
| 291 | Brockholes | Platform End. | 347 | N.A. | |
| 292 | N.A. | | 348 | Lockwood | South End of Station. |
| 293,294 | Stocksmoor | Platform Ends. | 349P | Preston (L. & Y.) | South End of Platform. |
| 295 | Penistone | Platform End. | 350 | N.A. | |
| 296 | Netherton | Station (Tunnel Top). | 351 | Blackpool (Talbot Road) | Station. |
| 297 | Dewsbury | Platform End. | 352 | N.A. | |
| 298 | Dewsbury | Goods Siding. | 353 | N.A. | |
| 299-302 | Goole (L. & Y.) | Loco Yard. | 354 | Hesketh Park | Station. |
| 303P | Goole (L. & Y.) | Mineral Sidings. | 355 | Barton | Station. |
| 304 | Goole (L. & Y.) | No. 2 Road to the 50-ton Crane to Coal Sdgs. | 356 | Preston (W.L.) | Goods Yard. |
| 305 | Brighouse | Between the Down North Line and the Up | 357 | N.A. | |
| 306 | Whitley Bridge | West End. [Goods Loop.] | 358 | N.A. | |
| 307 | Whitley Bridge | East End. | 359P | Fleetwood | Station. |
| 308 | Knottingley | Up Side (Doncaster Branch). | 360-363 | Accrington | Loco Shed. |
| 309 | Knottingley | Down Side (Goole Branch). | 364 | N.A. | |
| 310 | Knottingley | Down Side (Doncaster Branch). | 365P | Kirkham | Goods Yard. |
| 311 | N.A. | | 366P | Poulton | Platform End. |
| 312 | N.A. | | 367P | Wakefield | Turner's Lane Box. |
| 313 | Wakefield | Up Goods Loop, Granary Wall, West End. | 368P | Wakefield | Turner's Lane Box. |
| 314 | N.A. | | 369P | Wakefield | Goole Branch. |
| 315P | Wakefield | East End of Platform. | 370-374 | Wakefield | Loco Yard. |
| 316 | Wakefield | West End of Platform. | 375 | N.A. | |
| 317P | Elland | Station. | 376 | Bury (Bolton Street) | North End. |
| 318 | N.A. | | 377P | Hollins | Down Sidings. |
| 319P | Wakefield | East Box. | 378 | Birkdale | Station. |
| 320 | N.A. | | 379P | Wakefield | Calder Bridge. |
| 321P | Horbury Junction | Up Sidings. | 380P | Blackpool (Central) | Station. |
| 322 | N.A. | | 381P | Southport | Platform End. |
| 323,324 | Crigglestone | Ends of Platforms. | 382 | Atherton | Goods Yard. |
| 325 | Barnsley | Turntable Road. | 383 | N.A. | |
| 326-329 | Sandhills | Loco Yard. | 384 | Whitley Bridge | Outside Down Loop. |
| 330P | Bradford | Turntable (G.N. Side). | 385,386 | Leeds (Wortley) | Engine Shed. |
| 331P | Mill Hill | Station. | 387 | N.A. | |
| 332 | N.A. | | 388P | Blackburn | No. 3 Siding (E.L.). |
| 333,334 | Halifax North Bridge | Station. | 389 | Smithy Bridge | Up Sidings. |
| 335P | Brindle Heath | Goods Sidings. | 390 | Horbury and Ossett | East End of Horbury and Ossett Station Plat. |
| 336 | N.A. | | 391 | Smithy Bridge | Down Sidings. |

| | | | | | |
|---------|---------------------------|-----------------------------|---------|------------------------|------------------------------------|
| 392 | Manchester (Victoria) | New Extension. | 441 | Bury (Bolton Street) | Loco Shed. |
| 393 | Manchester (Victoria) | New Extension | 442 | Newton Heath | Loco Yard. |
| 394 | Manchester (Victoria) | East End. | 443,444 | Blackpool (Central) | Loco Sidings. |
| 395 | Manchester (Victoria) | New Extension. | 445 | N.A. | |
| 396 | Manchester (Victoria) | No. 7 Platform. | 446 | Blackpool (Central) | End of No. 4 Platform. |
| 397 | Manchester (Victoria) | East End. | 447-453 | Wigan | Loco Yard. |
| 398 | Bolton | Platform End. | 454 | N.A. | |
| 399 | Bolton | Near Platform End. | 455 | Bury (Knowsley Street) | |
| 400 | Bolton | Platform End. | 456P | Stansfield Hall | East |
| 401 | Bolton | Orlando Street Bridge. | 457 | N.A. | |
| 402 | Bolton | Rose Hill. | 458P | Hebden Bridge | East End of Station. |
| 403 | Bolton | Trinity Street Engine Pit. | 459 | Stansfield Hall | Down Line from Todmorden East Box. |
| 404 | Bolton | Platform End. | 460 | N.A. | |
| 405P | Low Moor | East End. | 461P | Newton Heath | Thorpe's Bridge. |
| 406 | Blackpool (Waterloo Road) | Down Platform. | 462 | Manchester (Victoria) | No. 12 Platform East. |
| 407 | Low Moor | Platform End. | 463 | Bankfield Goods | Coal Sidings. |
| 408 | Thornhill | West End. | | | |
| 409 | Lockwood | South End of Up Loop. | | | |
| 410 | N.A. | | | | |
| 411 | N.A. | | | | |
| 412 | Radcliffe | Goods Yard Sidings. | | | |
| 413 | N.A. | | | | |
| 414-416 | Rose Grove | Loco. | | | |
| 417P | Rose Grove | Gannow Junction. | | | |
| 418 | Rose Grove | West Box. | | | |
| 419P | Rose Grove | Up Exchange Sidings. | | | |
| 420P | Rose Grove | Down Exchange Sidings. | | | |
| 421 | Brindle Heath | Goods Sidings. | | | |
| 422 | N.A. | | | | |
| 423 | Whitefield | Goods Yard. | | | |
| 424P | Irlams-o'-th'Height | Carriage sidings. | | | |
| 425 | North Mersey | Goods Yard | | | |
| 426P | Pemberton | Winstanley Colliery Siding. | | | |
| 427P | Orrell East | East Box. | | | |
| 428 | Manchester (Victoria) | No. 12 Platform West. | | | |
| 429 | Manchester (Victoria) | No. 13 Platform East. | | | |
| 430 | Manchester (Victoria) | Nos 14 & 15 Platforms East. | | | |
| 431 | Manchester (Victoria) | Nos 16 & 17 Platforms East. | | | |
| 432P | Beswick Goods | Goods Yard | | | |
| 433 | N.A. | | | | |
| 434 | South Docks | Entrance to Yard. | | | |
| 435,436 | Colne | Loco Yard | | | |
| 437P | Bury (Knowsley Street) | | | | |
| 438 | N.A. | | | | |
| 439 | N.A. | | | | |
| 440 | Bacup | Loco Shed. | | | |



This plate came from Knottingley and was attached to a 'standard' water column. It was fastened on by just two screws and the back of the plate was curved to match the shape of the column.

Size—12" x 9 1/4"

| | | | | | |
|---------|---------------------------|--|---------|-------------------------------|-------------------------------------|
| 464P | Brindle Heath | Irlam Box. | 511 | Fazakerley Junction | Exchange Sidings, Kirkby End. |
| 465P | Stainland | Station Platform. | 512 | Southport | Goods Yard, Derby Road. |
| 466P | Featherstone | Down Loop. | 513 | Blackpool (Central) | Bloomfield Road Carriage Sidings. |
| 467P | Featherstone | West End of Up Loop. | 514P | Blackpool (Central) | Bloomfield Road Carriage Sidings. |
| 468 | N.A. | | 515 | N.A. | |
| 469 | Horbury Junction | West End of Station. | 516 | Liverpool (Gt. Howard Street) | Goods Yard. |
| 470 | N.A. | | 517 | North Mersey | Goods Yard. |
| 471 | Goole (L. & Y.) | High Level near 60-ton Coaling Hoist. | 518 | Darton | Silkstone Junction. |
| 472 | N.A. | | 519P | Southport | Passenger Station. |
| 473 | Fazakerley Junction | Coal Sidings. | 520 | N.A. | |
| 474P | Royton | Station. | 521 | Bury (Bolton Street) | Tottington Junction. |
| 475 | N.A. | | 522 | Healey Mills | West. |
| 476 | Shafton Junction | Near Shafton Junction Box. | 523 | Liverpool (Wapping) | Goods Yard. |
| 477 | Goole | Near Loco Coal Stage Tank. | 524 | N.A. | |
| 478 | Skelmersdale | Goods Yard. | 525 | Blackpool (Waterloo Road) | Goods Yard. |
| 479 | N.A. | | 526 | North Mersey | Goods Yard. |
| 480 | Wakefield | Down Loop, Calder Bridge to Oakenshaw Jct. | 527 | Miles Platting | Coal Stage, Philips Park. |
| 481 | Liverpool (Exchange Sta.) | Exchange Station, Turntable Road. | 528 | Todmorden | Coal Stage. |
| 482 | Knottingley | Loco Yard. | 529 | Miles Platting | Station. |
| 483 | Middleton Junction | Down Sidings. | 530 | Healey Mills | West. |
| 484 | N.A. | | 531 | Healey Mills | West. |
| 485 | Fleetwood | Station. | 532 | Ormskirk | Bay Siding, Coal Stage Road. |
| 486 | Bromley Cross | Goods Yard. | 533 | Newton Heath | Shunting Neck, Lightbourne Sidings. |
| 487,488 | Blackburn | Tunnel End. | 534P | Southport | Passenger Station. |
| 489 | Daisy Field | Great Harwood Junction. | 535P | Chorley | No. 1 Signal Cabin |
| 490 | N.A. | | 536 | N.A. | |
| 491 | N.A. | | 537,538 | Southport | Passenger Station. |
| 492 | N.A. | | 539 | Miles Platting | Collyhurst Street Sidings. |
| 493 | N.A. | | 540 | Blackburn | Down Platform. |
| 494 | N.A. | | 541 | Norton | South Side. |
| 495 | Miles Platting | Park Station End. | 542 | Brighouse | East End. |
| 496 | N.A. | | 543 | Brighouse | East End |
| 497 | Newton Heath | Lightbourne Sidings. | 544 | Brockholes | Down Platform. |
| 498 | Liverpool (Exchange Sta.) | Station No. 11 Road. | 545,546 | Bury (Knowsley Street) | Loco Yard. |
| 499 | Rochdale | Stage in Up Coal Sidings. | 547 | Mytholmroyd | Marshalling Sidings. |
| 500 | Westhoughton | Goods Yard | 548P | Mytholmroyd | Marshalling Sidings. |
| 501 | Southport | Carriage Sidings. | 549 | Radcliffe | End of Platform. |
| 502 | Bury (Knowsley Street) | Loop Junction. | 550 | N.A. | |
| 503 | Kirkby | Goods Yard Up Sidings. | 551P | Westhoughton | Near Chulsey Gate Cabin. |
| 504P | Newton Heath | Moston Exchange Sidings | 552P | Sowerby Bridge | End of Platform. |
| 505P | Bradley Fold | Down Loop. | 553 | Shepley and Shelley | Clayton West Junction. |
| 506P | Fazakerley Junction | Exchange Sidings, Kirkby End. | 554 | Shepley and Shelley | Clayton West Junction. |
| 507P | Haslingden | At End of Up Platform. | 555 | Hollinwood | Goods Yard. |
| 508P | Middleton Junction | Siding near Vitriol Works Box. | | | |
| 509 | Shafton Junction | Near Shafton Junction Box. | | | |
| 510P | Blackpool (Central) | Station Platform. | | | |

There were un-numbered columns at Colne, Doncaster, Goole (N.E.), Hillhouse, Huddersfield 2, Normanton 3, Preston (N.U.) 8 and Preston (L. & Y.).

NON-ALLOCATED NUMBERS & NOTES

REGISTER

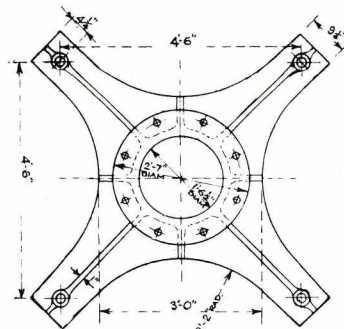
| | | | |
|---------|---|---------|---|
| 13 | Removed from Bury (E.L.) after January 1908. | 320P | Removed from Wakefield (Goole branch) after April 1902. |
| 31-34 | Removed from Manchester (Victoria) (East end of station) after April 1902. | 322 | Removed from Horbury Junction (Sidings) after April 1902. |
| 35 | Removed from Manchester (Victoria) (East end of station) after April 1902 and installed at Blackpool (Talbot Road). | 332 | Not included in 1902, 1908, or 1921 lists. |
| 38P | Removed from Radcliffe after January 1908. Replaced by No. 549. | 336 | Removed from Rose Grove (West Box) after April 1902. |
| 45-48 | Removed from Bolton after April 1902. | 340 | Removed from Salford (Hope Street) (Ship Canal Branch) after April 1902. |
| 49P | Removed from Darcy Lever after April 1902. Installed (without P) at Horbury and Ossett before January 1908. | 344 | Removed from Radcliffe (Goods Yard) after April 1902. Replaced by No. 412 before January 1908. |
| 59 | Removed from Bolton Station (Platform end) after April 1902. | 347 | Not included in 1902, 1908 or 1921 lists. |
| 60 | Not included in 1902, 1908 or 1921 lists. | 349P | Replacement for No. 73 at Preston (E.L.). |
| 73P | Removed from Preston (E.L.) after April 1902. Replaced by No. 349. | 350 | Removed from Manchester (Victoria) (West end Platform 6) after April 1902. |
| 75 | Installed Preston Junction after January 1908. | 352,353 | Removed from Southport (Central) (Platforms 1 and 3) after January 1908. |
| 89 | Not included in 1902, 1908 or 1921 lists. | 357,358 | Removed from Salford (Hope Street) after April 1902. |
| 97 | Removed from Wigan Station (East end) after January 1908. | 364 | Removed from Accrington (Loco Shed) after April 1902. |
| 103 | Removed from Wigan (Loco Yard) after April 1902. | 375 | Removed from Wakefield (Loco Yard) after January 1908. |
| 104 | Removed from Wigan (Loco Yard) after April 1902. At Bradford (Broomfield Sidings) (Water Tank) in January 1921 list. | 383 | Installed at Royton Junction after April 1902 and removed after January 1908. |
| 105 | Removed from Wigan (Loco Yard) after April 1902. | 387 | Installed at Haslingden after April 1902 and removed after January 1908. |
| 112 | Note this number used twice. | 410,411 | Not included in 1908 or 1921 lists. |
| 125 | Removed from Lytham after April 1902. | 413 | At Southport (Fork Carriage Sidings) in January 1908, later removed. |
| 127,128 | Removed from Blackpool (Talbot Road) (Loco Sidings) after April 1902. | 422 | Not included in 1908 or 1921 lists. |
| 133 | Removed from Blackpool (Central) (Loco Sidings) after April 1902. | 433 | At Liverpool (Gt. Howard Street) (Shunting neck) in January 1908, later removed. |
| 134 | Removed from Blackpool (Central) (Turntable Road) after April 1902. | 438,439 | At Walsden (Summit Tunnel East) in January 1908, later removed. |
| 138 | Probably removed from Fleetwood before April 1902. | 445P | At Blackpool (Central) (Bloomfield Carriage Sidings) in January 1908, later removed. |
| 148 | Removed from Liverpool (Exchange) after January 1908 and used at Sowerby Bridge. | 454 | Not included in 1908 or 1921 lists. |
| 153 | Removed from Sandhills (Loco Yard) after January 1908. | 457 | Not included in 1908 or 1921 lists. |
| 157 | Removed from North Mersey (Goods Yard) after January 1908. | 460 | Not included in 1908 or 1921 lists. |
| 160P | Removed from Aintree Sorting Sidings after January 1908. | 468 | Removed from Bury (E.L.) (near Buckley Wells Box) after Jan 1908. |
| 168 | Removed from Accrington (South) after January 1908. | 470 | Removed from Ormskirk (Down bay sidings) after January 1908. |
| 169,170 | Removed from Accrington (Loco Yard) after April 1902. | 472 | Removed from Fazakerley Junction (Exchange sidings shunting neck, Kirkby end) after January 1908. |
| 173 | Removed from Accrington (West) after January 1908. | 475 | 515 |
| 183 | Removed from Blackburn Station after January 1908. | 484 | 520 |
| 229 | Removed from Newton Heath (Loco Yard) after January 1908. | 490,491 | 524 |
| 248 | Removed from Low Moor (South End) after April 1902. | 492,493 | 532 |
| 265P | Removed from Sowerby Bridge (East End) after January 1908. | 494 | 536 |
| 267 | Removed from Sowerby Bridge (East End) after January 1908. | 496 | 550 |
| 292 | Removed from Brockholes (Down Line) after January 1908. | | |
| 304 | Installed at Goole (L & Y) (No. 2 road) after April 1902 and before January 1908. | | |
| 305 | Installed at Brighouse (Up goods loop) after April 1902 and before January 1908. | | |
| 311 | Removed from Knottingley (Loco Yard) after January 1908. | | |
| 312 | Removed from Knottingley (Depot) (Stores Yard) after January 1908. | | |
| 314 | Parachute at Wakefield (West End) in April 1902, column at Brighouse (Goods sidings) in January 1908. Not in 1921 list. | | |
| 318 | Not included in 1902, 1908 or 1921 lists. | | |

Addenda

| | |
|---------|---|
| 290P | Taken from Lockwood to Blackpool (Talbot Road) and replaced by No. 348 in 1897. |
| 189P | Installed at Blackburn Station in July 1886. |
| 62P,63P | Bullfield, included under Bolton in January 1921 list. |

Technical drawing of a circular mechanical component, likely a flange or end view of a pipe. The drawing includes a central assembly with a horizontal section line. Key dimensions and features are labeled:

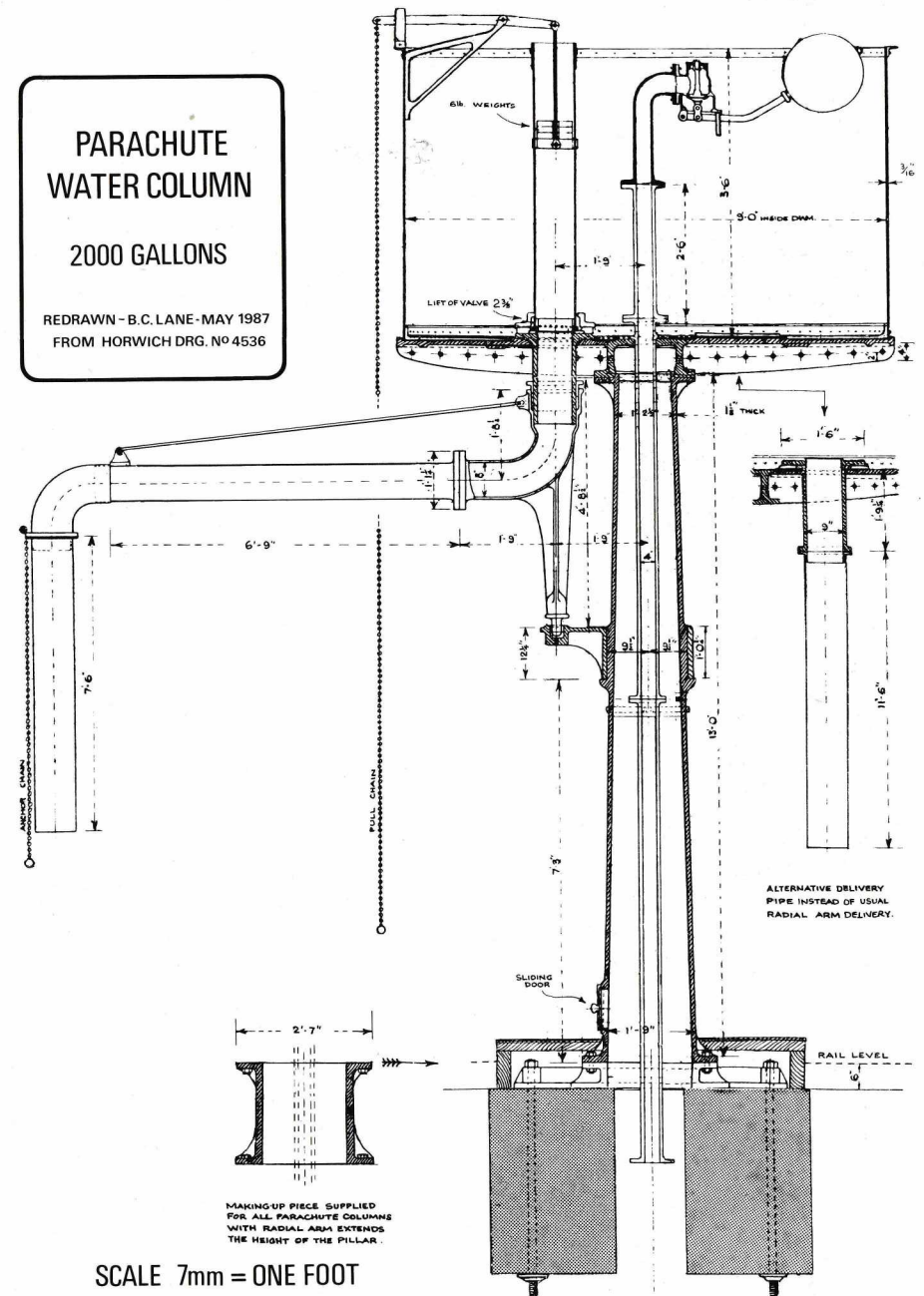
- Central Assembly:**
 - Two concentric circles on the left with a center-to-center distance of $1'-9"$.
 - A central rectangular block with a width of $4"$ and a height of $3'-0"$.
 - Two concentric circles on the right with a center-to-center distance of $1'-9"$.
 - A dashed line indicating a $9'-0"$ radius or distance from the center of the central block to the outer edge of the right-hand circles.
- Side View:** A rectangular view on the right showing a height of $2'-6"$.
- Overall Dimensions:**
 - A horizontal dimension of $1'-9"$ from the center of the left-hand circles to the center of the central block.
 - A horizontal dimension of $1'-9"$ from the center of the central block to the center of the right-hand circles.



A detailed technical line drawing of a vertical steam engine. The engine features a large, circular flywheel at the top, which is divided into two main sections by a vertical centerline. The flywheel has a complex internal structure with various pipes, valves, and a central vertical shaft. A long, horizontal crank arm extends from the center of the flywheel, ending in a long, vertical piston rod. The piston rod is connected to a large, vertical cylinder (the piston) which is mounted on a base. The base is a four-legged, star-shaped structure. A ladder is attached to the left side of the flywheel, and a chain is visible on the right side, likely for timing or adjustment. The drawing is signed 'B. CLANE' at the bottom right.

PARACHUTE WATER COLUMN

REDRAWN - B.C. LANE - MAY 1987
FROM HORWICH DRG. NO 4536



SCALE 7mm = ONE FOOT



A selection of WATER COLUMNS

UPPER LEFT—

A very early contractors column bearing an 1859 date plate. The location is Hebden Bridge and the LYR number is 276.

LOWER LEFT—

Included here because it shows much detail not shown elsewhere, this is another contractor-supplied column at Sowerby Bridge, No. 266. It originally had a swan-neck to the delivery arm and a ball-shaped balance weight on the near end. An identical column is shown in an 1870s photograph of Bolton station (Platform One, p.9). Note the 'water ticket' box on the column.

TOP RIGHT—

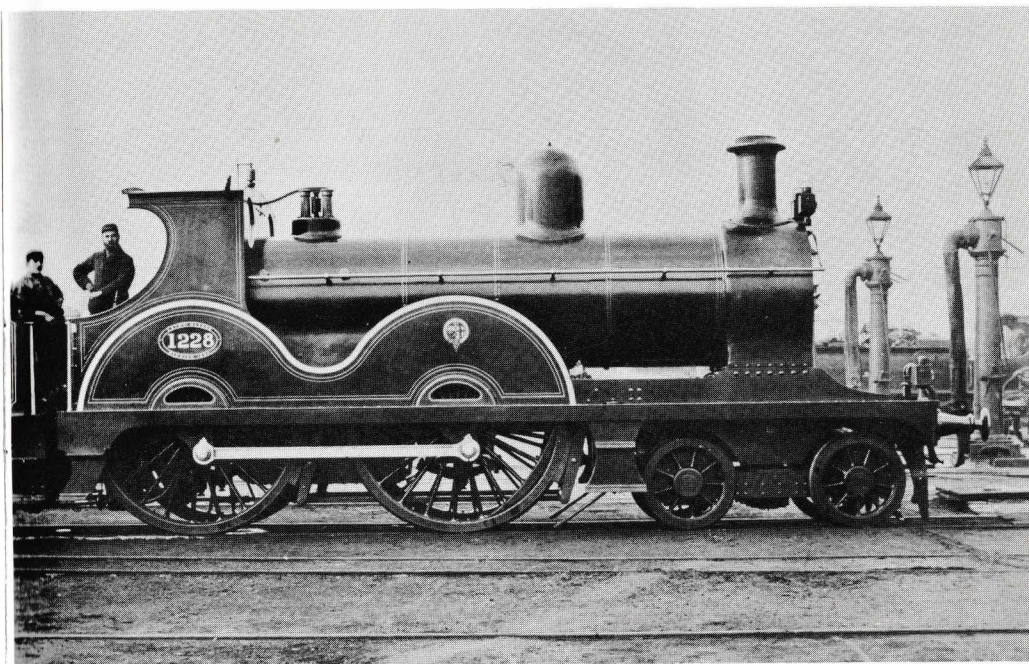
Aspinall 7'-3" 'Flyer' No. 1228 of 1894 stands in original condition at Newton Heath shed about the turn of the century. The water columns date from the late 1870s and were the pattern put in at most sites for at least a decade. The gas lamps on the top were altered (at this location) before the Great War to a simpler type as shown in 'LYR Loco Sheds' p.91.

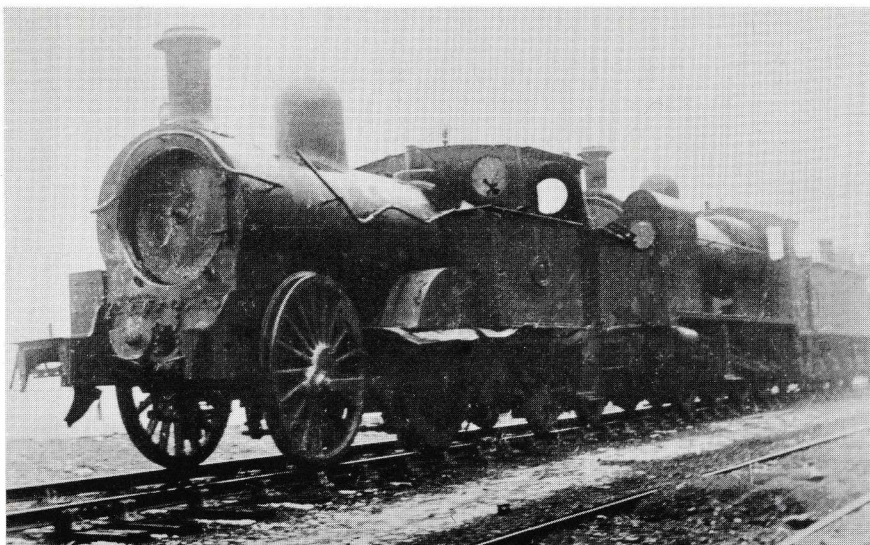
A new type was introduced in the 1890s as shown in our CENTRE PAGE photograph of Low Moor.

LOWER RIGHT—

The water tank at the north end of the platform at Lockwood had gas flues to guard against frost as evidenced by the vents above it. The LYR tabulated such tanks with parachute types like the one at the south end of the station (Platform 12, p.15) but in this location the official records show neither as a 'P' (parachute). This tank was No. 289 at the grouping but had been renumbered 87 by the late 1940s. The loco is No. 12325 (LYR 1303).

Photograph—D. Ibbotson





THE CRIPPLE AND THE COMPOUND

B. C. Lane

It was a Friday in mid January, the 22nd to be accurate, 1909, when two engines which had been overhauled at Horwich works were despatched back to their home shed. Typical of the time of year, a dense fog hung over Lancashire as the pristine engines started off on their journey home to Liverpool. They had not gone far on the line southward to Wigan before they were put into a spur at Hindley & Blackrod Junction to allow a passenger train from Chorley to Wigan to pass them. The density of the fog caused the error on the part of the engine crews and the locomotives were still foul of the main line when the passenger train hit them. One person died in the accident.

Eric Mason mentions the accident in his book (page 202) but is incorrect about the damaged engine. It was in fact No.2, a Sharp Stewart 0-4-4T built in 1885 as No.916 and re-numbered the following year. The engine stood in the works yard at Horwich for over a year before being officially withdrawn and one presumes that there was some reticence to write off a machine just after a thorough overhaul.

The engine was still there in the spring of 1910 when Compound 0-8-0 No.1476 was employed on road tests. An indicator shelter was fitted to the smokebox platform where one or two men would read the assortment of gauges employed in the test. It will be noted too that the 0-8-0 had one of the original narrow chimneys and the results of the testing (also carried out on other classes at this period) led to the alterations in draughting and the resultant wider chimneys.

The notice reproduced here gives the details of one of the runs with 1476. It should be remembered that in 1910, the railway was still using the four-wheeled Dynamometer Car pioneered by Aspinall in 1896. The distance from Horwich to Hellifield was only forty miles so the allowance of sixty-five minutes for the journey might at first appear to be generous.

The route though abounded with gradients and the three miles of 1 in 65 after Chorley with the marginally easier banks out of Blackburn and Whalley would severely test any engine. The average speed of nearly 40 mph by a goods locomotive with 4'6" wheels pulling a ten-coach train is a most creditable effort.

(M351)

Lancashire & Yorkshire Railway.
Passenger Superintendents Office.

Manchester 6th May 1910

SUPPLEMENTARY ADVICE OF SPECIAL TRAIN

MONDAY 9th May

No. 5 Special Train - Horwich Works to Hellifield

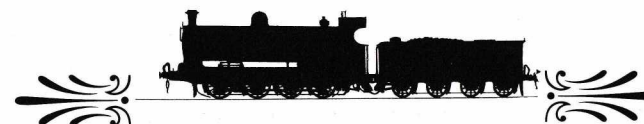
| | | |
|---------------|--------|---------|
| Horwich Works | depart | 9-40 am |
| Chorley | pass | 9-52 |
| Blackburn | " | 10-10 |
| Clitheroe | " | 10-22 |
| Hellifield | arrive | 10-45 |

returning as required

Train will consist of Compound Coal Engine 1476,
10 bogie coaches and Dynamometer Car.

Train must have a clear road. Carriages at Horwich.

J.P.Crouch.



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larger libraries. Excerpts have been used in 'Platform' from time to time. Members
are not advised to send any money to the above address after so many years.*

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