

The
VIRTUAL MUSEUM
of the
LANCASHIRE & YORKSHIRE RAILWAY

Operational Documents & Pamphlets.

1919 October

Use of Wrong Line Orders.

Illustrations and Rulings.

(40 Pages).

Lancashire and Yorkshire
Railway.

**USE OF
WRONG LINE
ORDERS.**

Illustrations and Rulings.

OCTOBER, 1919.

ARTHUR WATSON,
Manchester. General Manager.

419
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Lancashire and Yorkshire Railway.

USE OF
WRONG LINE
ORDERS.

—
Illustrations and Rulings.
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OCTOBER, 1919.

Manchester.

ARTHUR WATSON,
General Manager.

WRONG LINE ORDER FORMS.

| Form. | Colour. | Issued by | Issued to |
|---------|-------------|-----------------|------------|
| A | Pink | Guard | Signalman. |
| B | Green | Driver..... | Signalman. |
| C | Yellow..... | Signalman | Driver. |
| D | White | Guard..... | Driver. |

FORM A.

LANCASHIRE AND YORKSHIRE RAILWAY.

(A supply of these Forms must be kept by each Guard.)

(Pass 333.)

TO THE SIGNALMAN AT.....SIGNAL BOX.

Allow Driver of Engine No.....to return on the wrong Line to the remainder of
his Train standing on the.....Line at.....
I will prevent its being moved until the return of the engine.

Signed.....Guard.

Signed.....Bank Engine-driver.

Date.....19.....

Time issued.....m.

* Catch Points exist at.....

* The person taking this Order to the Signaller is responsible for noting the position
of Catch Points.

(GUARD'S WRONG LINE ORDER.)

3

RULE 221 (f).—If it be found necessary to return to the train or near portion of the train on the wrong Line, the Engine-driver must, before starting with the front portion, send his Fireman to the Guard to obtain his written authority to the Signalman at the nearest Signal-box in advance where the front portion of the train can be put away, authorising him to allow the engine to return from that point on the wrong Line, and without this authority the Signalman must not allow the engine to return on the wrong Line to its train. The Signalman must retain this Order. If there is a Bank Engine in the rear this authority must be countersigned by the Driver of that engine. Should there, however, be an intermediate Signal-box, the Order held by the Engine-driver must be retained by him and shown to the Signalman there when running in both directions, and given up to the Signalman at the place where the front portion of the train has been put away when he arrives there with the second portion. After giving such instructions, the Guard must continue to protect his train in the rear, and prevent a following train from pushing it ahead.

If there is an intermediate Signal-box provided with a Cross-over road between the point where the rear portion of the train is left on the running Line and the Signal-box to which the front portion of the train will have to go to be disposed of, the "Wrong Line" Order must be made out by the Guard to the Signalman at such intermediate Box, and the Driver, after leaving the Order with the Signalman there, must proceed to the place where he can dispose of his train, and then return on the proper line of rail to the intermediate Box and be there crossed on to the Line on which the rear portion of his train is standing, so that the engine shall not travel on the wrong Line further than is absolutely necessary.

FORM B.

LANCASHIRE AND YORKSHIRE RAILWAY.

LOCOMOTIVE DEPARTMENT.

(A supply of these Forms must be kept by each Engine-driver.)

(L 65.)

TO THE SIGNALMAN AT.....SIGNAL BOX.

Allow an Assistant Engine or a Breakdown Van Train to proceed on the wrong Line as authorised by Rule 221 (see other side), for my Train which is Stationary on theLine at..... I will not move my engine in any direction until the arrival of the Assistant Engine.

Signed.....Driver.

Date.....19.....

Time issued..... 111

* Catch Points exist at.....

* The person taking this Order to the Signalman is responsible for noting the position of Catch Points.

(30 - DRIVER'S WRONG LINE ORDER.)

5

RULE 221 (g).—When a train is brought to a stand on any Running Line, owing to the Failure of the engine or from any other cause, and the Guard has gone to the rear to protect it, it may be necessary for the engine coming to the assistance of the train, or for the Breakdown Van Train, to travel on the wrong Line from the Signal-box in advance. In such a case the Engine-driver of the disabled train must write out an authority for the Signaller at the Box in advance to allow the assistant engine, or the Breakdown Van Train, to travel on the wrong Line to the disabled train. The Fireman of the disabled train must hand the written authority to the Signaller, and accompany the assistant engine, or the Breakdown Van Train, to his train, advising the Engine-driver where, and under what circumstances, the disabled train is situated, and the Signaller must show the authority to the Driver before allowing the assistant engine or Breakdown Van Train to proceed on the wrong Line. The Engine-driver of the disabled train, after giving the order for the assistant engine, or the Breakdown Van Train, to run on the wrong Line, must not allow his train to be moved until the assistant engine or the Breakdown Van Train arrives, unless satisfactory arrangements have been previously made to prevent the assistant engine or the Breakdown Van Train from coming on the wrong Line, and his Fireman has returned and handed the “Wrong Line” Order back to the Engine-driver.

FORM C.

LANCASHIRE AND YORKSHIRE RAILWAY.

(A supply of these Forms must be kept in each Signal-box.)

(Pass 334.)

**AUTHORITY FOR ENGINE-DRIVER TO TRAVEL ON THE
WRONG LINE IN CASE OF ACCIDENT.**

To Driver of Engine No.....Working.....m. Train
from.....to.....

I authorise you to return with your train on the wrong Line to this Signal-box.

Signature of.....Signalman,

at.....Signal-box.

Date.....19.....

Time issued.....m.

Catch Points exist at.....

(SIGNALMAN'S WRONG LINE ORDER.)

RULE 222.—If, in case of accident, it is necessary for a train, or portion of a train, to return on the wrong Line to the Signal-box in the rear, the Guard or Fireman must first go or send some other competent person to the Signalman there, and obtain his permission in writing for the train, or portion of train, to run on the wrong Line to his Box; but the Engine-driver must not move in the wrong direction until he has received such written permission.

FORM D.

LANCASHIRE AND YORKSHIRE RAILWAY.

(A supply of these Forms must be kept by each Guard.)

(Pass 333a.)

TO THE DRIVER OF ENGINE No.....

I authorise you to set back to the rear portion of your train.

Signed.....Guard.

Signed.....Bank Engine-driver.

Date.....19.....

Time issued.....m.

Catch Points exist at.....

9
(GUARD'S WRONG LINE ORDER.)

RULE 221 (j).—If, after a train has become accidentally divided, both portions have been brought to a stand within sight of each other, and there is not a Signal-box near either end of the train, the front portion may be set back to the rear portion, provided the two portions can be re-coupled; but, before moving, the Engine-driver must send his Fireman to the Guard who is protecting the rear portion for a written authority to set back.

If there is a Bank Engine in the rear, this authority must be countersigned by the Driver of that engine.

RULE 217.

(a) When a train is stopped by accident failure, obstruction, or other exceptional cause (unless it has arrived at or passed the Home Signal), the Guard, if there be only one, or the Rear Guard, if there be more than one, must immediately go back at least three-quarters of a mile, unless he arrive at a Signal-box within that distance, plainly exhibiting his Hand Danger Signal, to stop any following train, and, in addition to his Hand Signals, he must take Detonators (to be used by day as well as by night), which must be placed upon the Line on which the stoppage has happened, as follows, viz:—

One Detonator a quarter of a mile from his train,

One Detonator half a mile from his train, and

Three Detonators, ten yards apart, not less than three-quarters of a mile from his train, and must also continue to exhibit his Hand Danger Signal to stop any coming train.

(b) If the Guard arrive at a Signal-box within or at about three-quarters of a mile from his train, he must place three Detonators on the Line opposite the Box, and must also

instruct the Signāman to keep his Signals at Danger to protect the Line which is obstructed. He must then return to his train or take such other steps as may be necessary to deal with the obstruction.

(e) The Detonators must not be taken up until intimation has been received that the obstruction has been removed; and when the "Is Line Clear" signal for the next train which has to pass through the Section has been accepted by the Signal-box in advance, the train must be stopped, and the Engine-driver must be advised of the circumstances, and instructed to travel cautiously through the Section.

(d) Should the distance of not less than three-quarters of a mile fall within a Tunnel, or close to the mouth of a Tunnel nearest to the obstruction, or in any other position where, owing to the formation of the Line, or to some other circumstance, the Engine-driver of an approaching train would be unable to obtain a good and distant view of the Hand Danger Signal, then, unless there be a Signal-box between the obstruction and the Tunnel, the Signal must be exhibited and Detonators must be placed on the Line at the end of the Tunnel

farthest from the obstruction, or at such a distance over and above the prescribed distance of not less than three-quarters of a mile as may be necessary to ensure the Engine-driver obtaining a good and distant view of such Signal.

(e) Before the Guard in going back enters a Tunnel, he must place three Detonators on the Line, ten yards apart, at the end of the Tunnel nearest to the obstruction.

(f) In order as quickly as possible to secure the safety of the Line as well as to obtain assistance and to regulate the working of the traffic, if the Signal-box in the rear of the obstruction be the nearer, the Guard, after protecting his train by Detonators, as directed above, must go to such Signal-box and advise the Signāman of the obstruction, but if the Signal-box in advance of the obstruction be the nearer or can be more quickly arrived at, the second Guard, if there be more than one Guard, or the Fireman, if there be not more than one Guard, must immediately go to the Signal-box and advise the Signāman of the cause of the obstruction.

(g) If the engine be able to run forward, it must be detached, and the Driver must proceed to the Signal-box in advance for the purpose

of carrying out the provisions of the previous paragraph, having, if required, first obtained from the Rear Guard an order to return on the Wrong Line, as provided for in Rule 221.

(b) If the Guard obtain assistance from the rear, he must ride on the engine of the assisting train, and point out to the Engine-driver the position of the disabled train. The assisting train must run at reduced speed, and great caution must be observed by all concerned.

(i) If the engine obtained from the rear has to return on the Wrong Line, the Guard must, before starting with the assisting engine, first obtain the prescribed "Wrong Line Order" in accordance with Rule 222.

(j) Except as above stated, the Guard must not return to his train until recalled by the Engine-driver sounding the whistle of his engine, and when recalled, he must leave the three most distant Detonators, and return to his train, taking up the other Detonators on his way. Should he be recalled before reaching the prescribed distance, he must then place on the rail three Detonators, ten yards apart, and return to his train, taking up the other Detonators on his way.

(k) Should the stoppage or failure occur to an engine not attached to a train, the Fireman must immediately go back and act in the way prescribed for the Guard.

(l) Where Parallel Lines are so near together that a vehicle running on either Line fouls the other, Detonators must be placed on each Line.

(m) When there are more than two Lines, and an accident causes more than one to be fouled, the necessary steps must be taken to protect all the Lines obstructed.

RULE 221.

(a) When a train or portion of a train is left on any Running Line from accident or inability of the engine to take the whole forward, or from any other cause, the Engine-driver must not return for it on the same Line, except as ordered in clauses (f) and (g) of this rule, but must cross on to, and travel along, the proper Line, and must re-cross at the nearest point behind the part left, which he must push before him until convenient to go in front again with the engine. If there be a Cross-over road immediately in front of the train, and the

operation can be performed within sight of the Signaller, the Driver may use such Cross-over road for the purpose of attaching his engine in front of the train.

(b) In cases where it is necessary to divide a Goods Train on an incline, owing to the inability of the engine to take the whole forward, both portions must, where practicable, be worked up the incline with a break-van in the rear. When a train is divided in this way at a Station, or at an intermediate Signa-box where a portion of the train can be disposed of, it will generally be found most convenient to shunt the front portion of the train into a Siding, and take the rear portion forward first (with the break-van attached in rear) to the next Station or Signal-box where there are means of disposing of it. After the first portion of the train has been disposed of, the engine must return on the proper Line, with the break-van, for the purpose of working forward the other portion of the train, which has been left behind, and the break-van must, in that case also, be attached in the rear.

(c) Unless both portions of the train are worked forward with a break-van in the rear a Tail Lamp must not be carried on the engine or last vehicle of the front portion of the train,

before reaching the first Signal-box, where the Engine-driver must stop and inform the Signaller of the position of matters; if the engine, or front portion of the train, has to pass into the next Section, a Tail Lamp must then be placed in the rear. The Signaller must not give "Train out of Section" to the Signal-box in the rear, until he has satisfied himself that the whole of the train has arrived.

After sunset, or in foggy weather or during falling snow, before the front portion is drawn forward a White Light must be exhibited on the front vehicle of the rear portion by the man who divides the train.

In the case of the train being accidentally divided, the Guard in charge of the rear portion must place a White Light on the leading vehicle of the rear portion of the divided train.

(d) When two Guards are employed with the train, the front Guard (or where both Guards ride in the rear, the under Guard) must uncouple it, and ride upon the last vehicle of the front portion; the other Guard, after putting on his break and securing the last portion of the train so that it will remain stationary, must go back and protect it in accordance with Rule 217.

(e) When there is only one Guard with the train the Fireman must ride upon the last vehicle of the front portion, and the Guard must take the necessary measures to protect the last portion.

(f) If it be found necessary to return to the train or rear portion of the train on the Wrong Line, the Engine-driver must, before starting with the front portion, send his Fireman to the Guard to obtain his written authority to the Signaller at the nearest Signal-box in advance where the front portion of the train can be put away, authorising him to allow the engine to return from that point on the Wrong Line (see Form A), and without this authority the signaller must not allow the engine to return on the Wrong Line to its train. The Signaller must retain this Order. If there is a Bank engine in the rear this authority must be countersigned by the Driver of that engine. Should there, however, be an intermediate Signal-box, the Order held by the Engine-driver must be retained by him and shown to the Signaller there when running in both directions, and given up to the Signaller at the place where the front portion of the train has been put away when he arrives there with the second portion. After giving

such instructions, the Guard must continue to protect his train in the rear, and prevent a following train from pushing it ahead.

If there is an intermediate Signal-box provided with a Cross-over road between the point where the rear portion of the train is left on the running Line and the Signal-box to which the front portion of the train will have to go to be disposed of, the "Wrong Line" Order must be made out by the Guard to the Signaller at such intermediate Box, and the Driver, after leaving the Order with the Signaller there, must proceed to the place where he can dispose of his train, and then return on the proper line of rail to the intermediate Box, and be there crossed on to the Line on which the rear portion of his train is standing, so that the engine shall not travel on the Wrong Line further than is absolutely necessary.

(g) When a train is brought to a stand on any Running Line, owing to the failure of the engine, or from any other cause, and the Guard has gone to the rear to protect it, it may be necessary for the engine coming to the assistance of the train, or for the Breakdown Van train, to travel on the Wrong Line from the Signal-box in advance. In such a case the

Engine-driver of the disabled train must write out an authority (on Form B where in use, see end of Rule) for the Signaller at the Box in advance to allow the assistant engine, or the Breakdown Van train, to travel on the Wrong Line to the disabled train. The Fireman of the disabled train must hand the written authority to the Signaller, and accompany the assistant engine, or the Breakdown Van train, to his train, advising the Engine-driver where, and under what circumstances, the disabled train is situated, and the Signaller must show the authority to the Driver before allowing the assistant engine or Breakdown Van train to proceed on the Wrong Line. The Engine-driver of the disabled train, after giving the order for the assistant engine or the Breakdown Van train to run on the Wrong Line, must not allow his train to be moved until the assistant engine or the Breakdown Van train arrives, unless satisfactory arrangements have been previously made to prevent the assistant engine or the Breakdown Van train from coming on the Wrong Line, and his Fireman has returned and handed the "Wrong Line" order back to the Engine-driver.

(h) Should there be any intermediate Signal-box without a Crossover-road, the

Fireman, when going for assistance, must show the written authority (on Form B where in use) to the Signaller at that Box.

(i) The Engine-driver, when returning for the portion of his train that has been left behind, or when pushing such portion of his train, or the Driver of the assistant engine or the Breakdown Van Train, as the case may be, must not pass any Signal-box without the permission of the Signaller.

(j) If, after a train has become accidentally divided, both portions have been brought to a stand within sight of each other, and there is not a Signal-box near either end of the train, the front portion may be set back to the rear portion, provided the two portions can be recoupled; but, before moving, the Engine-driver must send his Fireman to the Guard who is protecting the rear portion for a written authority to set back. (See Form D.)

If there is a bank engine in the rear this authority must be countersigned by the Driver of that engine.

RULE 222.

If, in case of accident, it is necessary for a train, or portion of a train, to return on the Wrong Line to the Signal-box in the rear, the Guard or Fireman must first go or send some other competent person to the Signalman there, and obtain his permission in writing for the train, or portion of train, to run on the Wrong Line to his box; but the Engine-driver must not move in the wrong direction until he has received such written permission (see Form C). This "Wrong Line Order" must be collected by the Signalman, and sent to the Station-master with his report.

RULE 223.

When moving in the wrong direction, as laid down in Rules 221 and 222, the Engine-driver must send his Fireman on foot in front of the engine to warn anyone who may be on the Line that the engine or the Breakdown Van train is approaching on the wrong Line. The Engine-driver must then proceed cautiously, travel at reduced speed, and make frequent use of the engine whistle.

ILLUSTRATIONS AND RULINGS.**RULE 217.**

Train stopped by Accident, Failure, or Obstruction.

A train is stopped in a long section as per clause (a) of Rule 217. The quickest means of obtaining assistance is by the engine being detached and running to advance box, to afterwards return on Wrong Line. What should be done?

**Ruling.**

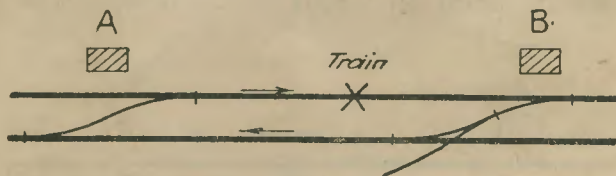
Before starting, the Driver should obtain a Wrong Line Order from the Guard as per clause (g). It should be the Pink Form "A," *vide* Rule 221 (f).

The order to be retained by the Signalman.

USE OF WRONG LINE ORDERS.

RULE 217.

A train is stopped in a section as per paragraph (a). The quickest means of obtaining assistance is by sending an engine from the rear box, to return to the rear box in accordance with the provisions of clause (i). What order should be issued?



Ruling.

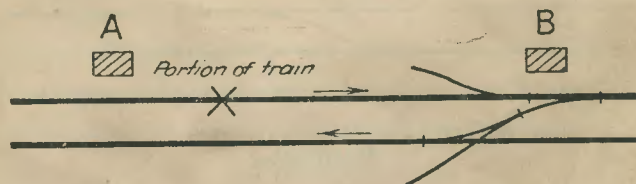
Before starting from the rear box the Driver should obtain the Signalman's Wrong Line Order Form "C" (yellow).

This order to be given back to the Signalman on the return of the engine to the rear box.

USE OF WRONG LINE ORDERS.

RULE 221.—Clause (f).

A train has to be divided in the section between "A" and "B," and it is necessary for the front portion to go forward to "B" and for the engine to afterwards return from that point to the rear portion. What order should be issued?

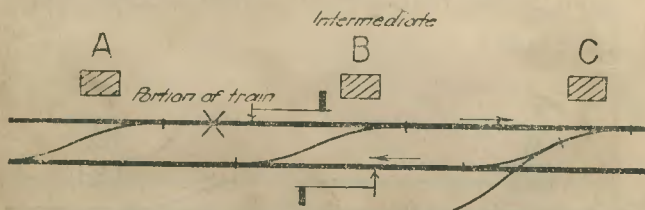


Ruling.

Before going forward to the advance box the Engine Driver should obtain from the Guard Wrong Line Order Form "A" (pink). This form should be handed to the Signalman at "B" and retained by him.

RULE 221.—Clause (f).

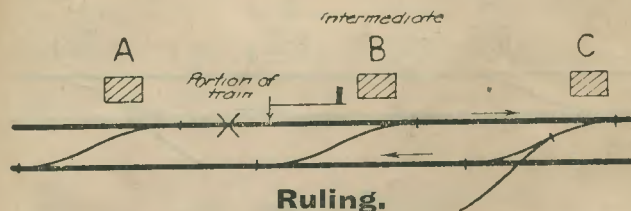
A train has to be divided in the section between "A" and "B" and the front portion taken to "C" to be placed in a siding. What order should be issued?

**Ruling.**

The Driver must obtain from the Guard Form "A" (pink), made out to the Signaller at "B," and this must be handed to the Signaller at "B" before proceeding to "C." After putting the front portion in the siding at "C" the engine must return on the right line to "B," cross over at that point, and return on wrong line to rear portion.

RULES 221 (Clause f) and 222.

A train becomes divided in the section between "A" and "B" and the front portion is brought to a stand at "C." The sections are short, and using the right line between "C" and "B" would cause considerable delay. Must the Guard make out the Wrong Line Order in such a case to the Signaller at "C" instead of "B" for the engine and front portion to return on the wrong line to the rear portion from "C"?

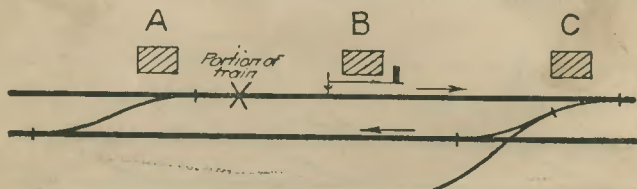
**Ruling.**

No. The Guard should in the above case make out the Wrong Line Order Form "A" (pink) to the Signaller at "B" for the engine with front portion of train to return from that box on the wrong line to the rear portion of the train, and the Engine-driver must obtain from the Signaller at "B" Wrong Line Order Form "C" (yellow) authorising him to return on the wrong line to that box in accordance with the provisions of Rule 222, such Wrong Line Order to be shown to and countersigned by the Signaller at "C." The first order should be left with the Signaller at "B," and the second order should be given up to the Signaller at "B" after returning to that box on the wrong line.

USE OF WRONG LINE ORDERS.

RULE 221.—(Clause f).

A train leaves the rear portion between "A" and "B" and requires to proceed to "C" to dispose of whole or part of front portion at "C," afterwards returning on wrong line to the rear portion of its train. There is no cross-over road at "B." What order should be issued?



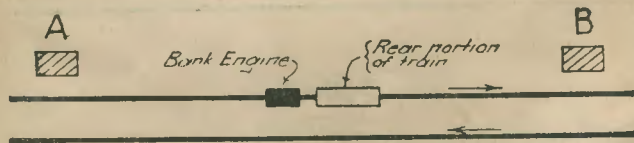
Ruling.

The Guard should make out Wrong Line Order Form "A" (pink) to the Signalman at "C." The Driver to shew the order to the Signalman at "B" when running in both directions, afterwards giving it up to the Signalman at "C" on arrival with second portion.

USE OF WRONG LINE ORDERS.

RULE 221.—Clause (f).

A train with a bank engine in rear becomes divided, the front portion going forward and is disposed of, the rear portion becoming stationary, the bank engine cannot move it, and it is necessary for the train engine to return on the wrong line for the rear portion of its train. Who should issue the order, the Guard or the Driver of the bank engine?

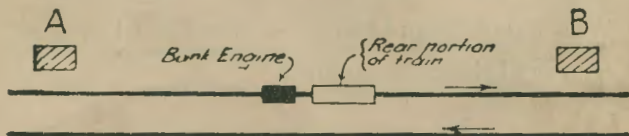


Ruling.

The Guard; but the order should be countersigned by the Driver of the bank engine.

RULE 221.—Clause (f).

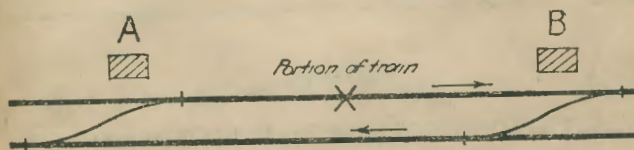
Instead of the train engine returning for the rear portion it is found advisable to allow another engine to remove it. May the order made out to the Signaller be utilised for the other engine?

**Ruling.**

Yes; but the number of the engine inserted on the Wrong Line Order should be altered, and the Driver made thoroughly to understand the position of the rear portion. The alteration should be made by the Driver of the train engine and be countersigned by him.

RULE 221 (g).

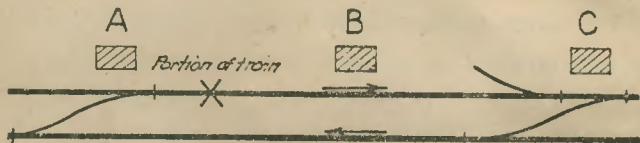
An engine fails between "A" and "B." Assistance is required from "B." What should be done?

**Ruling.**

The Driver must send his Fireman to "B" with Wrong Line Order Form "B" (green). The Signaller at "B" will retain this order and shew it to the Driver of the assistant engine.

RULE 221 (g).

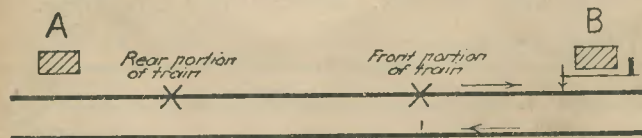
An engine fails between "A" and "B" and assistance is required from "C." There is no cross-over road at "B." What should be done?

**Ruling.**

The Driver should send his Fireman to Box "C" with Wrong Line Order "B" (green), made out to the Signaller at "C." The Fireman should shew the Order to the Signaller at "B" when proceeding to "C" for assistance. He should shew it to the Signaller at "C" and "B" when travelling in the wrong direction with the assisting engine, and afterwards give it up to the Signaller at "C" on arrival with the disabled engine and train.

RULE 221 (j).

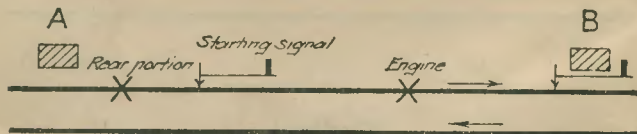
A train has become divided in a section and both portions are brought to a stand with front portion not arrived at "B's" Home Signal. What should be done?

**Ruling.**

The Driver should send his Fireman to the Guard and obtain Wrong Line Order Form "D" (white), made out to the Driver, authorising him to set back to rear portion. The Driver to retain the order.

RULE 221 (j).

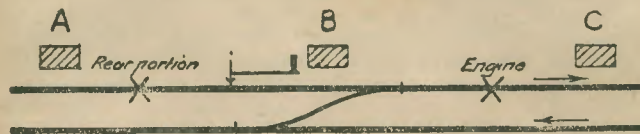
A train becomes divided and leaves the rear portion between Box "A" and the Starting Signal or just in advance of box where there is no Starting Signal, and the front portion is brought to a stand before reaching the Home Signal for "B" Box. What Wrong Line Order should be issued?

**Ruling.**

The Guard should issue Wrong Line Order "D" (white) to Driver to return to rear portion.

RULES 221 and 222.

A train becomes divided and the rear portion comes to a stand between "A" and "B." The engine or engine with front portion is brought to a stand in advance of "B's" last stop Signal. What orders should be issued?

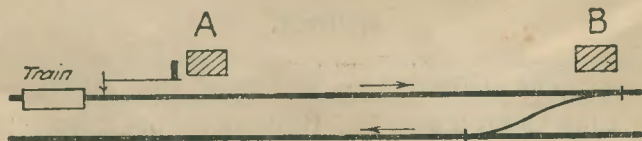
**Ruling.**

The Signalman at "B" should issue Wrong Line Order Form "C" (yellow) to the Driver, authorising him to return to his box. The Guard should issue Wrong Line Order Form "A" (pink) to the Signalman at "B," authorising the engine to return to rear portion. Both orders to be retained by the Signalman at "B."

USE OF WRONG LINE ORDERS.

RULE 222.

A train is brought to a stand at the Home Signal at "A," and owing to the engine having become short of water it is detached and runs forward to "B" for water. There is no cross-over road at "A," and it is necessary for the engine to return to its train on the wrong line, who should issue the Wrong Line Order?

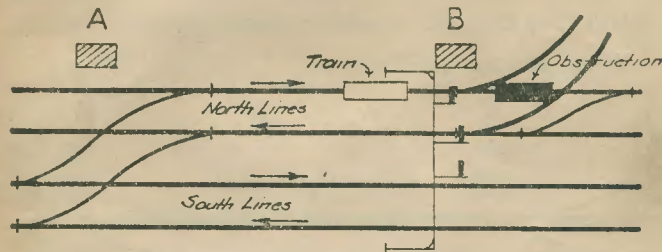
**Ruling.**

The Signalman, in accordance with the provisions of Rule 222.

USE OF WRONG LINE ORDERS.

RULE 222.

A train is standing on the North Line at "B" Home Signal when a mishap takes place in advance. It is necessary for the train to be set back to "A" Box. How should this be done?

**Ruling.**

The Signalman at "A" should issue Wrong Line Order Form "C" (yellow) to Driver to set back to "A." This order should be shown to and countersigned by Signalman at "B" before the setting back operations are carried out. The order to be retained by the Signalman at "A."

USE OF WRONG LINE ORDERS.

RULE 222.

A train which has passed the last Stop Signal becomes disabled or unable to proceed, and it is decided to set the train back to "A." What should be done?



Ruling.

The Signalman at "A" should issue Wrong Line Order Form "C" (yellow) and send it by the Guard or Fireman to the Driver of the train. On returning to the box the Driver, should deliver up the Order to the Signalman, who must retain it.

USE OF WRONG LINE ORDERS.

ELECTRIC TABLET REGULATIONS.

What are the circumstances, if any, under which a Wrong Line Order should be issued?

Ruling.

A Wrong Line Order should be issued when it is necessary for the train engine to be set back to the rear portion of its train in carrying out the provisions of clause (c) of the Electric Tablet Regulations 14B, *i.e.*, in the case of an engine assisting a train in the rear failing and it is necessary for the Driver of the train engine to return for the remainder of the train.

